



THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

Anthony Lee Washington III

2025.01.03 17:27:46-06'00'

PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED AND SEALED. THE SIGNATURE MUST BE VERIFIED ON THE ELECTRONIC DOCUMENTS.

HDR ENGINEERING, INC.  
120 BRENTWOOD COMMONS WAY  
SUITE 525  
BRENTWOOD, TN 37027  
ANTHONY L. WASHINGTON, III, P.E. 119749

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET .....	ROADWAY-SIGN1
TITLE SHEET .....	1
ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS .....	1A
PROJECT COMMITMENTS.....	1B
ESTIMATED ROADWAY QUANTITIES .....	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE .....	2A, 2A1
GENERAL NOTES.....	2B
SPECIAL NOTES.....	2C, 2C1
ENVIRONMENTAL NOTES.....	2D, 2D1
TABULATED QUANTITIES .....	2E

YEAR	PROJECT NO.	SHEET NO.
2025	NH-15(229)	ROADWAY-SIGN 1

**STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION**

# SIGNATURE SHEET



THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:  
**Anthony Lee Washington III**  
**2025.01.27 13:26:10-06'00'**  
PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED AND SEALED, THE SIGNATURE MUST BE VERIFIED ON THE ELECTRONIC DOCUMENTS.

HDR ENGINEERING, INC.  
120 BRENTWOOD COMMONS WAY  
SUITE 525  
BRENTWOOD, TN 37027  
ANTHONY L. WASHINGTON, III, P.E. 119749

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET .....	ROADWAY-SIGN2
ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS .....	1A
ESTIMATED ROADWAY QUANTITIES .....	2

YEAR	PROJECT NO.	SHEET NO.
2025	NH-15(229)	ROADWAY-SIGN 2

REV. 01-27-25: ADDED SHEET.

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

SIGNATURE  
SHEET

Index Of Sheets  
SEE SHEET NO. 1A

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
BUREAU OF ENGINEERING

GILES COUNTY

STATE ROUTE 15  
FROM LANIER ROAD (L.M. 18.39)  
TO LINCOLN COUNTY LINE (L.M. 25.22)

RESURFACE  
D OVERLAY, SPOT REPAIRS, AND PAVEMENT MARKING

STATE HIGHWAY NO. 15 F.A.H.S. NO. 64

28S015-F8-002  
END PROJECT NO. NH-15(229) RESURFACE  
L.M. 25.22

CSX OVERHEAD CROSSING [643077N]  
CSXT M.P. 223.91  
L.M. 23.63

BRIDGE DECK REPAIR PROJECT NO. 28S015-M3-003  
SR-15@ L.M. 18.39 LT & RT, L.M. 23.45, &  
L.M. 23.63 LT & RT

28S015-F8-002  
BEGIN PROJECT NO. NH-15(229) RESURFACE  
L.M. 18.39

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

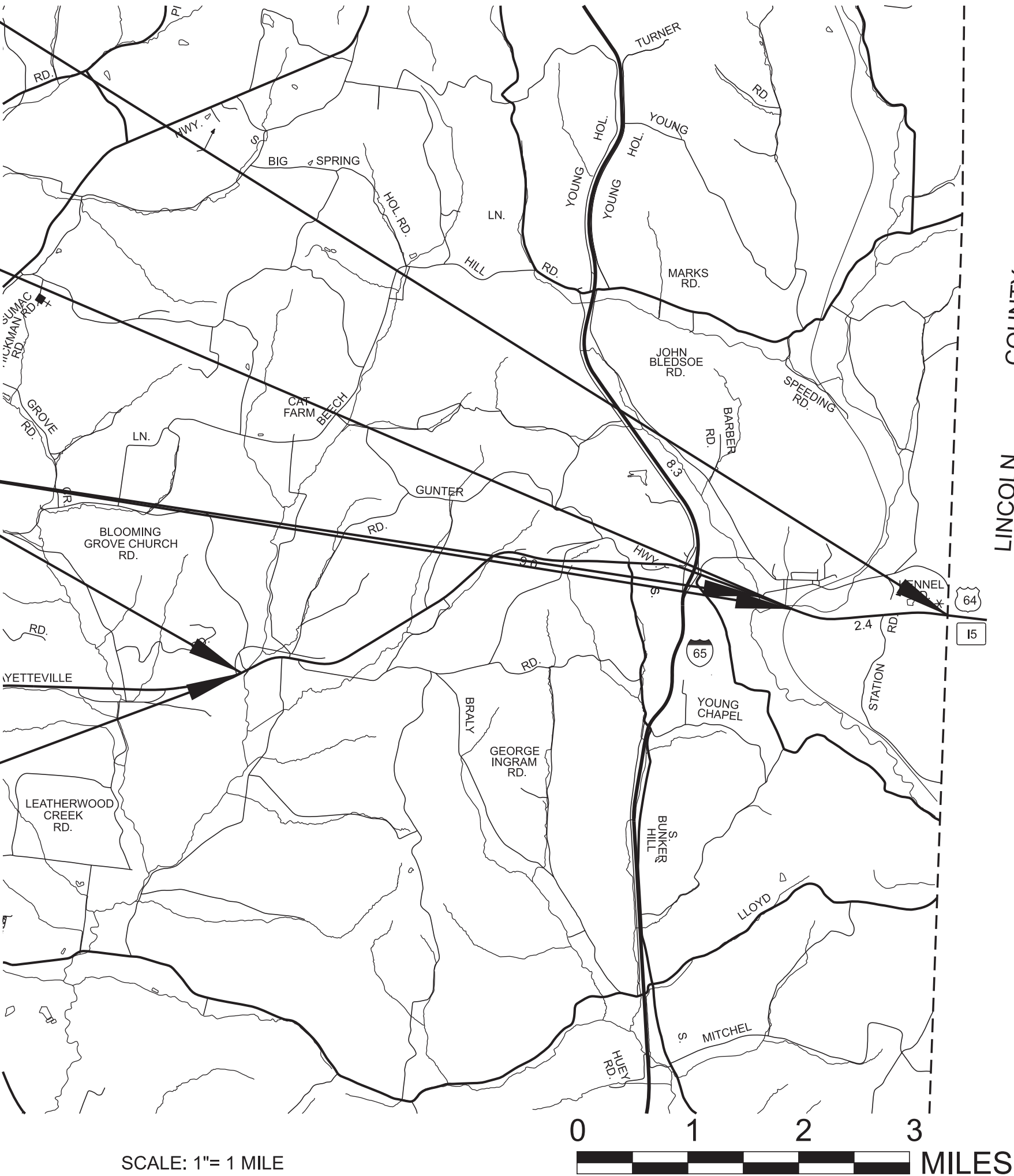
TDOT PROJECT MANAGER : KONNER SPRADLIN, P.E.

DESIGNED BY : HDR ENGINEERING, INC.

DESIGNER : ANTHONY L. WASHINGTON, III, P.E. CHECKED BY DAVID HORNE, P.E.

P.E. NO. 98034-4299-04 (DESIGN)

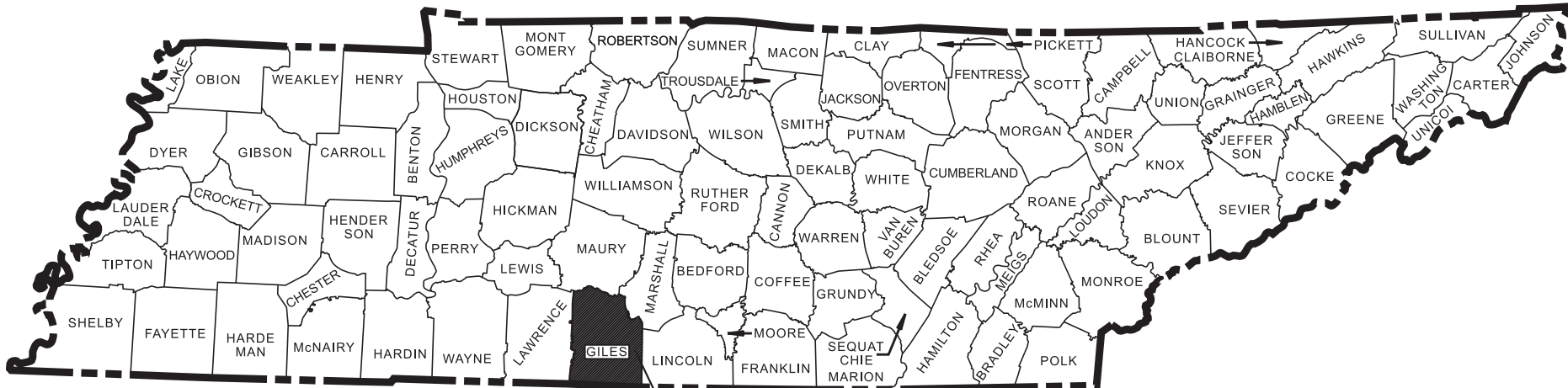
PIN NO. 115572.00



PROJECT LENGTH 6.83 MILES  
TOTAL LANE MILES RESURFACED 27.49 MILES

DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES	NO X

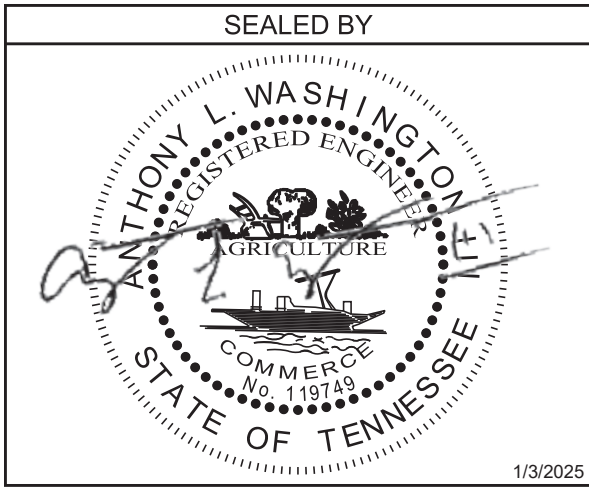
TENN.	YEAR	SHEET NO.
	2025	1
FED. AID PROJ. NO.	NH-15(229)	
STATE PROJ. NO.	28S015-F8-002	
STATE PROJ. NO.	28S015-M3-003	



PROJECT LOCATION

BRIDGE ID. # 28SR0150051 28SR0150052 28SR0150029  
28SR0150031 28I00650032 28I00650031  
28SR0150059 28SR0150061 28SR0150062  
28SR0150053 28SR0150054

NO EXCLUSIONS



APPROVED:   
WILL REID, CHIEF ENGINEER

DATE:

APPROVED:   
HOWARD H. ELEY, COMMISSIONER

TRAFFIC COUNTER  
AND  
WEATHER STATIONS

STATION	LOG MILE
TCS84	22.447
TCS35	23.254
TCS160	24.005

TRAFFIC DATA

ADT (2025)	5956
POSTED SPEED LM 18.39 - 22.48	65 MPH
POSTED SPEED LM 22.48 - 23.56	45 MPH
POSTED SPEED LM 23.56 - 24.23	55 MPH
POSTED SPEED LM 24.23 - 25.22	65 MPH

U.S. DEPARTMENT OF TRANSPORTATION  
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: \_\_\_\_\_  
DIVISION ADMINISTRATOR DATE



ROADWAY INDEX

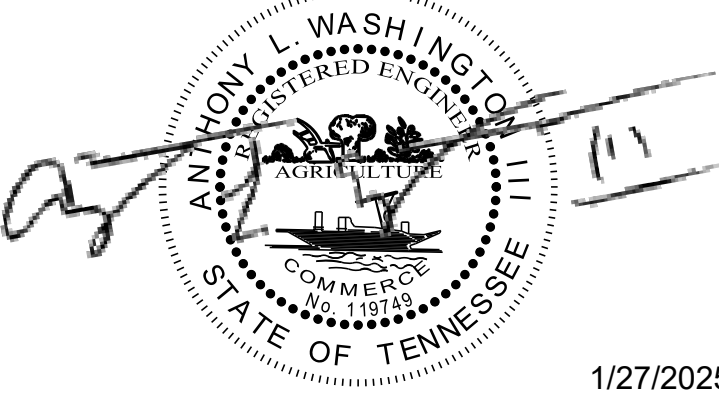
STANDARD ROADWAY DRAWINGS

SHEET NAME	SHEET NO.	DWG.	REV.	DESCRIPTION
SIGNATURE SHEET .....	ROADWAY-SIGN1	<b>STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS</b>		
SIGNATURE SHEET .....	ROADWAY-SIGN2			
TITLE SHEET .....	1	RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS .....	1A	RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
PROJECT COMMITMENTS .....	1B	RD-L-1	02-20-20	STANDARD LEGEND
ESTIMATED ROADWAY QUANTITIES .....	2	RD-L-1A		STANDARD LEGEND
TYPICAL SECTIONS AND PAVEMENT SCHEDULE .....	2A, 2A1	<b>DESIGN - TRAFFIC CONTROL</b>		
GENERAL NOTES.....	2B			
SPECIAL NOTES.....	2C, 2C1	T-M-1	06-28-19	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
ENVIRONMENTAL NOTES.....	2D, 2D1	T-M-2	01-09-24	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
TABULATED QUANTITIES .....	2E	T-M-3	07-07-23	MARKING STANDARDS FOR TRAFFIC ISLANDS, PAVED SHOULDERS AND MEDIANS FOR CONVENTIONAL ROADS
BRIDGE PLANS.....	B1 THRU B10	T-M-4	07-17-20	STANDARD INTERSECTION PAVEMENT MARKINGS
NOTE: THE ALPHABETICAL LETTERS “I”, “O”, & “Q” ARE NOT USED IN THE NUMBERING OF SHEETS  NO UTILITY SHEETS INCLUDED IN THIS SET OF PLANS		T-M-15A	06-28-19	ASPHALT SHOULDER RUMBLE STRIP INSTALLATION DETAILS FOR NON-ACCESS CONTROLLED ROUTES
		T-M-16	07-30-24	RUMBLE STRIPE INSTALLATION LAYOUT
		T-M-18	10-29-21	FLEXIBLE DELINEATOR DETAILS
		T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
		T-WZ-11	03-04-21	ONE LANE CLOSURE DETAIL ON DIVIDED HIGHWAYS
		T-WZ-12	03-04-21	ONE LANE CLOSURE DETAIL FOR BRIDGES ON DIVIDED HIGHWAYS
		T-WZ-FAB1		FLASHING YELLOW ARROW BOARD

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-15(229)	1A

REV. 01-27-25: ADDED ROADWAY-SIGN 2 TO INDEX.

SEALED BY



1/27/2025

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

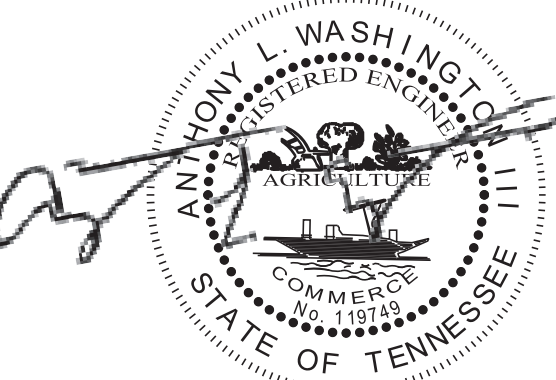
ROADWAY INDEX  
AND  
STANDARD  
ROADWAY  
DRAWINGS



TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-15(229)	1B

PROJECT COMMITMENTS			
COMMITMENT ID	SOURCE DIVISON	DESCRIPTION	STATION / LOCATION
EDHZ001	ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS	ASBESTOS CONTAINING MATERIAL (ACM) SURVEYS WERE COMPLETED ON BRIDGE NO. 28SR0150051 SR-15 EB OVER BUCHANAN CREEK AND LANIER RD LM 18.39 (28-SR015-18.39R) AND BRIDGE NO. 28SR0150052 SR-15 WB OVER BUCHANAN CREEK AND LANIER RD LM 18.39 (28-SR015-18.39L). NO ASBESTOS WAS DETECTED. PLEASE SEE THE REPORTS FOR FURTHER DETAILS AND PHOTOGRAPHS. NO SPECIAL ACCOMMODATIONS FOR DEMOLITION AND WASTE DISPOSAL ARE ANTICIPATED FOR THESE STRUCTURES AND THE MATERIAL CAN BE DEPOSITED IN A C&D LANDFILL. PRIOR TO THE DEMOLITION OR REHABILITATION OF ANY STRUCTURE (BRIDGE OR BUILDING), THE CONTRACTOR IS REQUIRED TO SUBMIT THE NATIONAL EMISSION STANDARDS FOR HAZARDOUS AIR POLLUTANTS STANDARD 10-DAY NOTICE OF DEMOLITION TO THE TDEC DIVISION OF AIR POLLUTION CONTROL (PER TDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (JANUARY 1, 2021) SECTIONS 107.08.D AND 202.03).	BRIDGE NO. 28SR0150051 SR-15 EB OVER BUCHANAN CREEK AND LANIER RD LM 18.39 (28-SR015-18.39R) AND BRIDGE NO. 28SR0150052 SR-15 WB OVER BUCHANAN CREEK AND LANIER RD LM 18.39 (28-SR015-18.39L)

SEALED BY



1/3/2025

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

PROJECT  
COMMITMENTS

ESTIMATED ROADWAY QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY 28S015-F8-002
(1)	208-01.05 BROOMING & DEGRASSING SHOULDERS	L.M.	24.6
(2)	303-01 MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON	9909
(3)	403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	76
(4)(5)	411-01.21 LONGITUDINAL JOINT SEALANT	L.M.	32
(6)	411-02.10 ACS MIX(PG70-22) GRADING D	TON	20275
	411-12.02 SCORING SHOULDERS (NON-CONTINUOUS) (16IN WIDTH)	L.M.	11.9
	411-12.03 SCORING FOR RUMBLE STRIPE (NON-CONTINUOUS) (8IN WIDTH)	L.M.	9.5
(7)	415-01.01 COLD PLANING BITUMINOUS PAVEMENT	TON	2500
(8)	712-01 TRAFFIC CONTROL	LS	1
	712-04.01 FLEXIBLE DRUMS (CHANNELIZING)	EACH	75
(9)	712-06 SIGNS (CONSTRUCTION)	S.F.	2731
	712-08.03 ARROW BOARD (TYPE C)	EACH	2
	713-02.14 FLEXIBLE DELINEATOR (WHITE)	EACH	20
	713-16.01 CHANGEABLE MESSAGE SIGN UNIT	EACH	2
	716-01.21 SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR) (1 COLOR)	EACH	245
	716-01.22 SNOWPLOWABLE RAISED PAVMENT MARKERS (MONO-DIR)(1 COLOR)	EACH	186
	716-01.23 SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR)(2 COLOR)	EACH	774
	716-01.30 REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER	EACH	1205
(10)(11)	716-02.04 PLASTIC PAVEMENT MARKING(CHANNELIZATION STRIPING)	S.Y.	32
(10)(11)(12)	716-02.05 PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	325
(10)(11)	716-02.06 PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	12
	716-02.12 PLASTIC PAVEMENT MARKING (8IN LINE)	L.M.	0.2
(13)	716-05.20 PAINTED PAVEMENT MARKING (6" LINE)	L.M.	3.7
	716-08.20 REMOVAL OF PAVEMENT MARKING (LINE)	L.M.	1.6
(11)	716-09.86 CONTRAST PAVEMENT MARKING 6"	L.M.	1.6
(11)	716-12.02 ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.	31.9
	717-01 MOBILIZATION	LS	1

THERE IS NO GUARDRAIL WORK ON THIS PROJECT

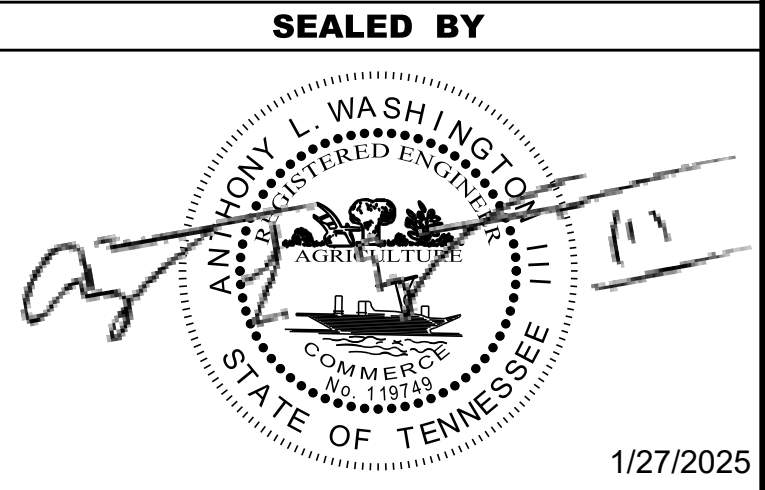
THERE ARE NO UTILITY ADJUSTMENTS ON THIS PROJECT

FOOTNOTES

- (1) TO BE USED AS DIRECTED BY THE ENGINEER. NO DIRT OR DEBRIS TO BE LEFT ON SHOULDER, THIS WORK TO BE PERFORMED BEFORE ALL OTHER OPERATIONS.
- (2) TO BE USED AS DIRECTED BY THE ENGINEER.
- (3) INCLUDES 4 TONS FOR PRIVATE DRIVES, BUSINESS ENTRANCES, FIELD ENTRANCES, COUNTY ROADS, CROSS OVERS AND INTERSECTIONS.
- (4) USE CRAFCO PAVEMENT JOINT ADHESIVE #34524 OR PAVON JOINT ADHESIVE BY PAVON CORPORATION.
- (5) TO BE USED FOR SEALING OF ALL SURFACE LAYER CONSTRUCTION JOINTS ALONG THE TRAVEL LANES AND SHOULDERS AS DIRECTED BY THE ENGINEER.
- (6) INCLUDES 915 TONS FOR PRIVATE DRIVES, BUSINESS ENTRANCES, FIELD ENTRANCES, COUNTY ROADS, CROSS OVERS AND INTERSECTIONS.
- (7) TO BE USED AS DIRECTED BY THE ENGINEER FOR BEGIN AND END OF PROJECT LIMITS, THE BEGIN AND END OF BRIDGES AT L.M. 18.39, L.M. 23.45, L.M. 23.52, AND L.M. 23.63, AND AT CURB AND GUTTER SECTIONS FROM L.M. 22.90 TO L.M. 23.37.
- (8) THE CONTRACTOR SHALL COMPLY WITH SECTION 712 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION REGARDING TEMPORARY TRAFFIC CONTROL AND THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (9) THE CONTRACTOR IS RESPONSIBLE FOR THE STAKING OF CONSTRUCTION SIGNS. IN THE EVENT THAT A CONSTRUCTION AND/OR REGULATORY SIGN IS TEMPORARILY DESIGNATED NOT IN USE DURING THE CONSTRUCTION PHASE OF A PROJECT, THE CONTRACTOR SHALL CHOOSE A SIGN COVERING APPROVED BY THE ENGINEER. TEMPORARY SIGN COVERINGS SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 712-06 SIGNS (CONSTRUCTION)
- (10) THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
- (11) ITEM TO BE USED AS PERMANENT MARKING ONLY
- (12) ALL STOP BARS ON SIDE STREETS WILL BE RETRACED AS DIRECTED BY THE ENGINEER.
- (13) ITEM TO BE USED AS TEMPORARY PAVEMENT MARKING ONLY.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-15(229)	2

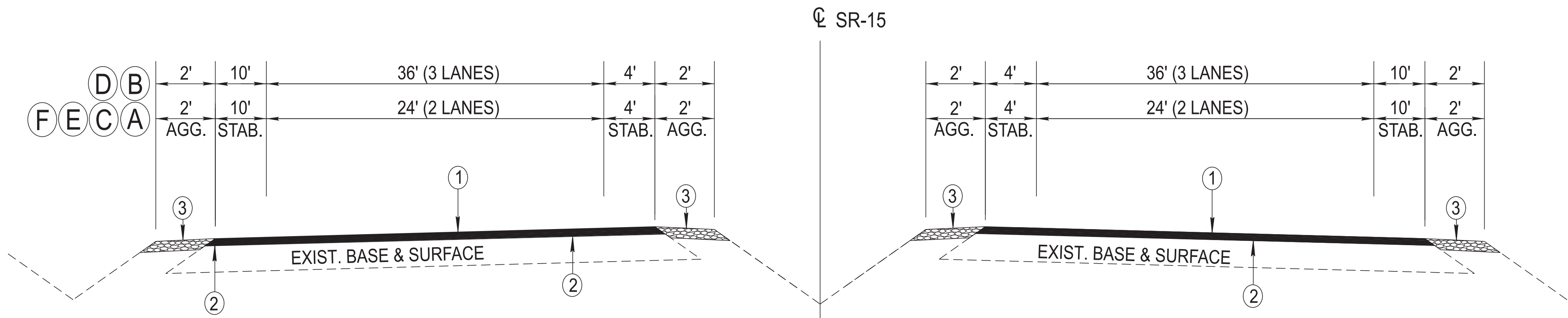
REV. 01-27-25: REMOVED PAY ITEM 712-02.60.



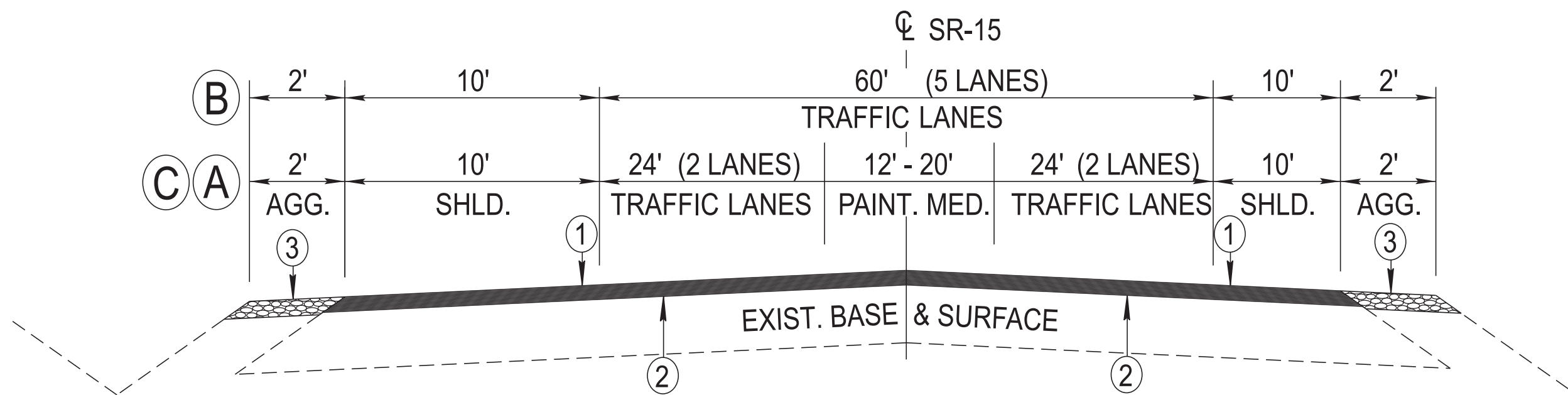
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

ESTIMATED  
ROADWAY  
QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-15(229)	2A

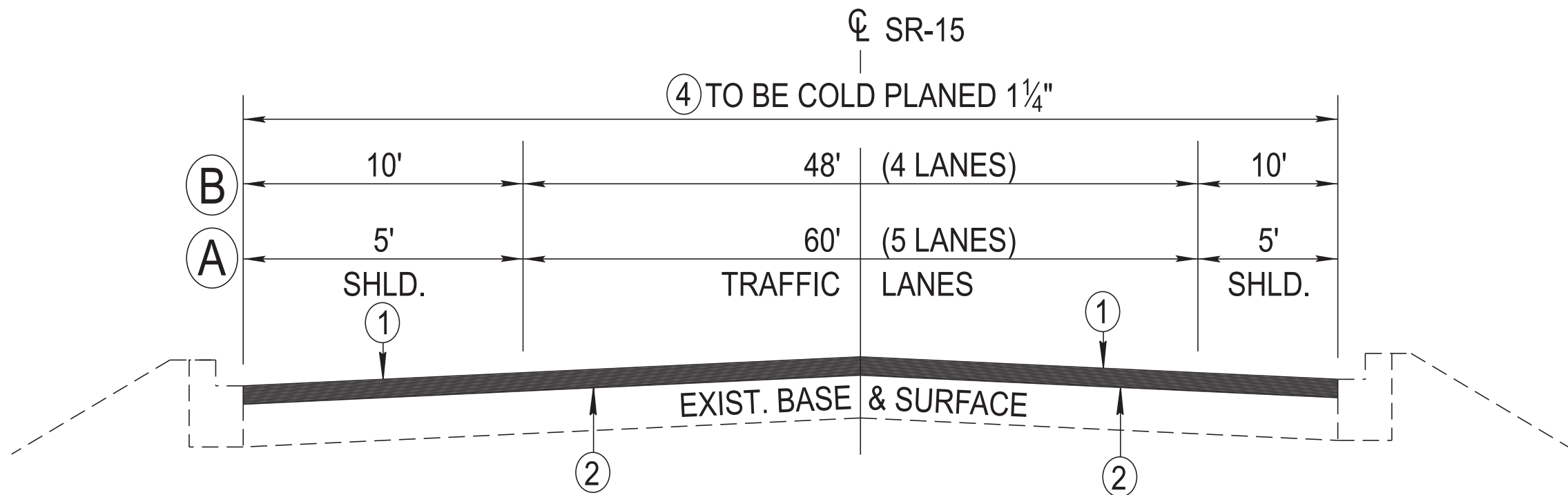


- STATE ROUTE 15
- (A) FROM: L.M. 18.39 TO L.M. 18.80
  - (B) FROM: L.M. 18.80 TO L.M. 18.86
  - (C) FROM: L.M. 18.86 TO L.M. 21.59
  - (D) FROM: L.M. 21.59 TO L.M. 21.65
  - (E) FROM: L.M. 21.65 TO L.M. 22.50
  - (F) FROM: L.M. 23.49 TO L.M. 25.22

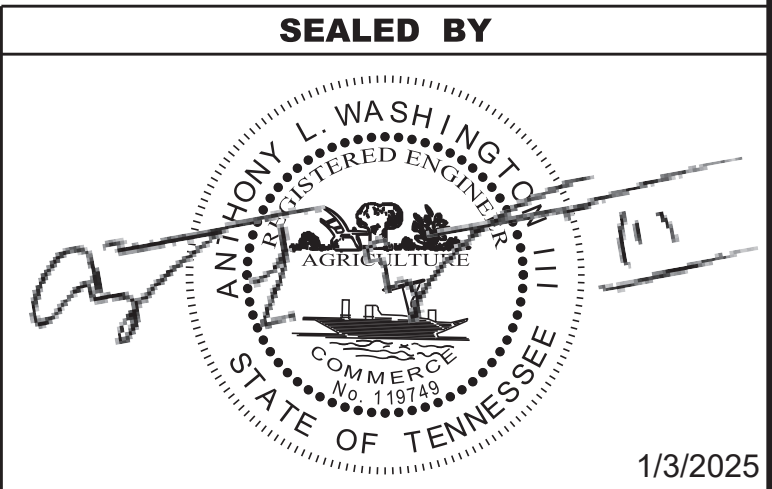


- SR-15  
TYPICAL SECTION
- (A) FROM: L.M. 22.50 TO L.M. 22.57
  - (B) FROM: L.M. 22.57 TO L.M. 22.90
  - (C) FROM: L.M. 23.37 TO L.M. 23.49

SEE SHEET 2A1 FOR PAVEMENT SCHEDULE



- SR-15  
TYPICAL SECTION
- (A) FROM: L.M. 22.90 TO L.M. 23.00
  - (B) FROM: L.M. 23.00 TO L.M. 23.37



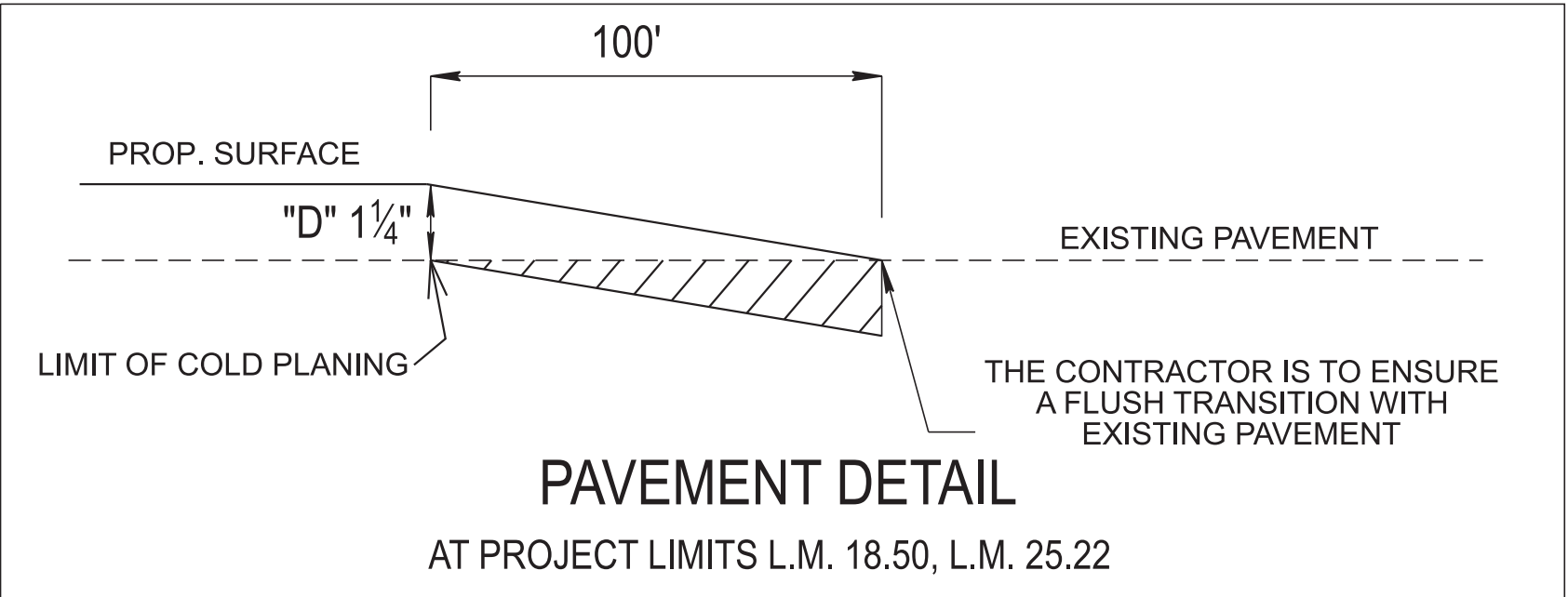
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

TYPICAL  
SECTIONS AND  
PAVEMENT  
SCHEDULE



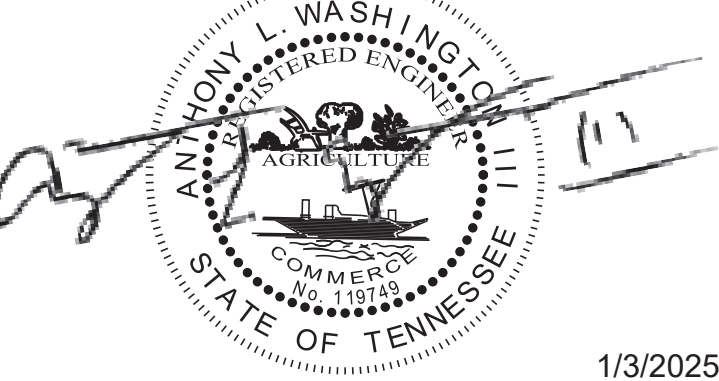
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-15(229)	2A1

BRIDGE DECK RECOMMENDATIONS (RESURFACING)				
BRIDGE NUMBER	LOCATION LOG MILE	CROSSES OVER/UNDER	BRIDGE LENGTH	BRIDGE DECK RECOMMENDATIONS
28SR0150051	18.39 R	BUCHANAN CREEK & LANIER RD.	596.75'	PROVIDE DECK REPAIR ITEM - SEAL WITH TYPE 1 THIN EPOXY OVERLAY (BRIDGE SHEETS INCLUDED)
28SR0150052	18.39 L	BUCHANAN CREEK & LANIER RD.	596.75'	PROVIDE DECK REPAIR ITEM - SEAL WITH TYPE 1 THIN EPOXY OVERLAY (BRIDGE SHEETS INCLUDED)
28SR0150029	20.070	BRANCH	27'	PAVE WITH PLANS MIX/ TREATMENT TYPE
28SR0150031	20.990	BRANCH	27'	PAVE WITH PLANS MIX/ TREATMENT TYPE
28I00650032	22.830	I-65 SB LANES	16.3'	
28I00650031	22.830	I-65 NB LANES	22.7'	
28SR0150059	23.450	LITTLE BRADSHAW CREEK	172.75'	PROVIDE DECK REPAIR ITEM - SEAL WITH TYPE 1 THIN EPOXY OVERLAY (BRIDGE SHEETS INCLUDED)
28SR0150061	23.52 R	OVERFLOW	124'	COLD PLANE 1.25" OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" OF NEW ASPHALT
28SR0150062	23.52 L	OVERFLOW	124'	COLD PLANE 1.25" OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" OF NEW ASPHALT
28SR0150053	23.63 R	BRADSHAW CREEK & CSXT R.R.	882'	PROVIDE DECK REPAIR ITEM - SEAL WITH TYPE 1 THIN EPOXY OVERLAY (BRIDGE SHEETS INCLUDED)
28SR0150054	23.63 L	BRADSHAW CREEK & CSXT R.R.	957'	PROVIDE DECK REPAIR ITEM - SEAL WITH TYPE 1 THIN EPOXY OVERLAY (BRIDGE SHEETS INCLUDED)



PROPOSED PAVEMENT SCHEDULE	
①	ASPHALTIC CONCRETE SURFACE (HM) 1¼" TH. (APPROX. 132.5 LBS/SY) ITEM NO. 411-02.10 ACS MIX (PG70-22) GRADING D
②	TACK COAT (TC) ITEM NO. 403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC) SEE D.G. 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD.
③	MINERAL AGGREGATE BASE (SHOULDERS) 6" TH. ITEM NO. 303-01 MINERAL AGGREGATE, TYPE A BASE, GRADING D
④	COLD PLANING 1¼" TH. (APPROX. 131.3 LBS/SY) ITEM NO. 415-01.01 COLD PLANING BITUMINOUS PAVEMENT

SEALED BY



1/3/2025

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

TYPICAL  
SECTIONS AND  
PAVEMENT  
SCHEDULE



GENERAL NOTES

GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (3) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

MISCELLANEOUS

- (2) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES AND POSTS WHERE AND AS DIRECTED BY THE ENGINEER. COST TO BE INCLUDED IN PRICE BID FOR OTHER CONSTRUCTION ITEMS.
- (3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

- (2) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.20, PAINTED PAVEMENT MARKING (6" LINE), L.M.

FINAL PAVEMENT MARKING

- (6) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:

a. SHOULDERS SHALL BE BROOMED AND DE-GRASSED AND MATERIAL SHALL BE PICKED UP AND REMOVED. THIS WILL BE PAID FOR UNDER ITEM NO. 208-01.05.

b. REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (9) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

SNOWPLOWABLE REFLECTIVE PAVEMENT MARKERS

- (20) REMOVE EXISTING SNOWPLOWABLE MARKERS PRIOR TO PAVING AND/OR COLD PLANING. REMOVE ALL ADHESIVES PRIOR TO PAVING. PATCH ANY HOLES OR DIVOTS RESULTING FROM THE REMOVAL OF A MARKER IN A MANNER WHICH ENSURES A UNIFORM PAVED SURFACE. PATCH WORK SHALL BE INCLUDED WITH COST OF OTHER ITEMS OF CONSTRUCTION..

PAVEMENT

PAVING

- (2) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

- (4) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (5) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- (6) PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND BUSINESS ENTRANCES WILL BE RESURFACED A PAVER WIDTH (LANE WIDTH) AS A MINIMUM. A PAVEMENT TAPER TO TRANSITION THE NEW PAVEMENT SHALL BE REQUIRED, IT SHALL BE BASED ON AN ADDITIONAL ONE FOOT OF WIDTH PER ONE INCH DEPTH OF PAVEMENT. IF THE SHOULDER IS NARROW ENOUGH THAT THE SUM OF THE SHOULDER AND THE TRANSITION ARE LESS THAN A PAVER WIDTH, THE TRANSITION SHALL OCCUR WITHIN THE PAVER WIDTH. IF THE SUM OF THE SHOULDER AND THE TRANSITION IS GREATER THAN A PAVER WIDTH (LANE WIDTH), THE TRANSITION SHALL OCCUR OUTSIDE OF THE PAVER WIDTH.
- (7) ON CURB AND GUTTER SECTIONS, PUBLIC ROAD INTERSECTIONS SHALL BE RESURFACED TO THE END OF RADIUS. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD SHALL BE PROVIDED.
- (8) ON URBAN TYPICAL SECTIONS, (CURB AND GUTTER), RESIDENTIAL DRIVEWAYS AND BUSINESS ENTRANCES SHALL HAVE A MINIMUM WIDTH OF MATERIAL NOT LESS THAN ONE FOOT USED IN THE TRANSITION TO FEATHER THE PAVEMENT EDGE.
- (9) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

SIGNING

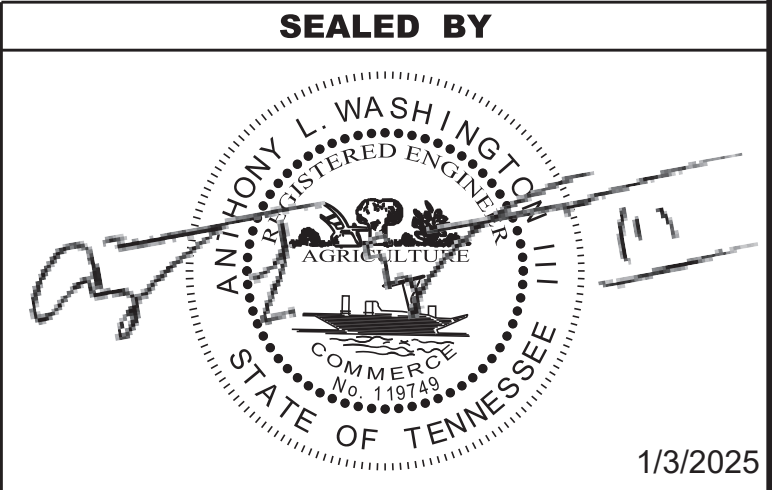
- (12) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-15(229)	2B



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

GENERAL  
NOTES



SPECIAL NOTES

MISCELLANEOUS

- (1)

THE CONTRACTOR SHALL TAKE EXTREME CARE WHEN WORKING AROUND EXISTING GUARDRAIL, HIGHWAY SIGNS, OR ANY DEPARTMENT OF TRANSPORTATION STRUCTURE TO AVOID DAMAGE DURING THE COURSE OF CONSTRUCTION. ANY DAMAGE WILL BE REPLACED OR REPAIRED AT HIS OWN EXPENSE.
- (2)

ALL SIGNING AND PAVEMENT MARKINGS ARE TO BE IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIGORM TRAFFIC CONTROL DEVICES

PAVEMENT MARKINGS

- (2)

THE CONTRACTOR IS TO PROVIDE THE LAYOUT OF ALL EXISTING PAVEMENT MARKINGS BEFORE BEGINNING THE RESURFACING PROJECT. EXISTING MARKINGS SHALL BE REFERENCED AS NEEDED TO ENSURE THAT ALL NEW MARKINGS ARE REPLACED IN THEIR CORRESPONDING LOCATIONS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

PAVEMENT

PAVING

- (4)

SURFACE TO BE CROWNED AS DIRECTED BY THE ENGINEER.

RESURFACING

- (5)

DURING COLD PLANING OPERATION, A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION WILL REMAIN OPEN.
- (6)

AFTER ANY COLD PLANING OPERATION, TEMPORARY STRIPING WILL BE REQUIRED BEFORE RETURNING ANY TRAFFIC ONTO EXCAVATED ROADWAY.

BRIDGE

- (1)

THE CONTRACTOR SHOULD TAKE EXTREME CARE WHEN COLD PLANING THE EXISTING ASPHALT OFF BRIDGE DECK, SO AS NOT TO DAMAGE THE EXISTING DECK SEALANT AND/OR EXPANSION JOINT MEMBERS (STEEL PLATES, BARS, AND/OR HEADERS). IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL BE RESPONSIBLE FOR NECESSARY REPAIRS ON ALL DAMAGED MEMBERS TO THE SATISFACTION OF THE PROJECT ENGINEER AT NO ADDITIONAL COST.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1)

ANY WORK REQUIRING LANE CLOSURES INCLUDING PAVEMENT MARKING OPERATIONS SHALL BE DONE AT NON-PEAK HOURS (9:00 A.M. TO 3:00 P.M. OR 7:00 P.M. TO 6:00 A.M.) UNLESS OTHERWISE DIRECTED BY THE TDOT OPERATIONS DISTRICT ENGINEER.
- (2)

THE CONSTRUCTION SIGNS, FLAGMEN AND OTHER TRAFFIC CONTROL DEVICES WILL BE IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (3)

ONE ADVANCE ROAD CONSTRUCTION AHEAD SIGN IS TO BE PLACED IN ALL LOCAL SIDE ROADS PRIOR TO THEIR INTERSECTION WITH THE PROPOSED PROJECT. LOCATION TO BE DETERMINED IN THE FIELD BY THE ENGINEER OR CONTRACTORS' SUPERINTENDENT.

WORK RESTRICTIONS

- (1)

CONSTRUCTION ACTIVITIES SHALL NOT INTERFERE WITH SCHOOL OR WORK TRAFFIC EITHER AM OR PM OR AS DIRECTED BY THE ENGINEER.

JOINT SEALANTS

- (1)

THE CONTACT SURFACE OF TRANSVERSE JOINTS AND LONGITUDINAL JOINTS IN THE SURFACE LIFT SHALL BE SEALED BY APPLYING JOINT SEALANT PRIOR TO PLACEMENT OF ADDITIONAL ASPHALT AGAINST THE PREVIOUSLY PLACED MATERIAL. MANUFACTURER'S RECOMMENDATIONS SHALL BE FOLLOWED IF THE MATERIAL NEEDS TO BE RE-HEATED, AND WHEN PLACING THE THIN, UNIFORM LAYER.
- (3)

PRIOR TO APPLICATION OF THE SEALANT, THE FACE OF THE JOINT SHALL BE THOROUGHLY DRY AND FREE FROM DUST OR ANY OTHER MATERIAL THAT WOULD PREVENT PROPER SEALING. ALL JOINTS SHALL BE SWEEPED OR BLOWN FREE OF LOOSE MATERIAL DIRT, VEGETATION, AND OTHER DEBRIS BY MEANS OF COMPRESSES AIR OR A POWER SWEEPER.
- (4)

TRUCK AND VEHICLE TRAFFIC SHALL NOT DRIVE ACROSS A SEALED JOINT UNTIL IT HAS DRIED SUFFICIENT TO PREVENT DAMAGE FROM TRACKING.

UTILITIES

- (2)

UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3)

THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCION.
- (4)

PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATION AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5)

THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

- A.

**DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:**
1.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:
- a.

WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
- b.


DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
- c.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
- d.

WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-15(229)	2C

SEALED BY



1/3/2025

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

SPECIAL  
NOTES



SPECIAL NOTES (CONT.)

CSXT SAFETY NOTES

- (1)

COMPLY WITH THE CONSTRUCTION SUBMISSION CRITERIA OF THE CSXT PUBLIC INFORMATION DOCUMENT AND CONSTRUCTION REQUIREMENTS OF THE PUBLIC PROJECTS MANUAL, WHICH IS AVAILABLE AT THE FOLLOWING URL: <https://www.csx.com/index.cfm/about-us/property/>
- (2)

ALL WORK IN THE FRA RED ZONE (WITHIN 4 FEET FROM OUTSIDE OF THE RAIL ON EACH SIDE OF THE TRACK) WILL BE DONE ONLY WITH A CSXT, FRA QUALIFIED FLAGMAN OR WATCHMAN AS SPECIFIED BY THE LOCAL ENGINEERING REPRESENTATIVE.
- (3)

ALL WORK BEYOND 4 FEET FROM THE OUTSIDE RAILS AND WITHIN 25 FEET MUST BE DONE UNDER THE SUPERVISION OF A QUALIFIED INSPECTOR OR CSXT FLAGMAN.
- (4)

CERTAIN TYPES OF WORK DONE BEYOND 25 FEET FROM THE OUTSIDE OF THE RAILS, AND WITH EQUIPMENT THAT WILL NOT REACH BEYOND THIS POINT, MAY BE DONE WITHOUT FLAGGING PROTECTION OR A WATCHMAN. THIS MUST BE APPROVED BY THE LOCAL ENGINEERING REPRESENTATIVE, THE AREA MUST BE PROTECTED BY A CONSTRUCTION FENCE, AND THE WORK MUST BE STATIONARY.
- (5)

ALL WORKERS WILL REMAIN OFF THE TRACKS. IF NECESSARY TO PERFORM THE WORK ON TRACK, PROTECTION WILL BE PROVIDED AS STATED ABOVE.
- (6)

ALL WORKERS MUST COMPLY WITH FEDERAL, STATE, AND LOCAL LAWS AND REGULATIONS, INCLUDING BUT NOT LIMITED TO THOSE OF THE OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) AND THE FEDERAL RAILROAD ADMINISTRATION (FRA).
- (7)

AT LEAST THIRTY (30) DAYS ADVANCED NOTIFICATION MUST BE GIVEN TO THE RAILROAD REPRESENTATIVE, TO SCHEDULE A RAILROAD FLAGMAN.
- (8)

THE CONTRACTOR MAY NOT USE CSXT RIGHT-OF-WAY, FOR STORAGE OF MATERIALS OR EQUIPMENT, WITHOUT PRIOR WRITTEN APPROVAL FROM CSXT.
- (9)

THE CONTRACTOR SHALL CONDUCT ITS WORK AT ALL TIMES, IN A MANNER WHICH WILL PROTECT CSXT’S PROPERTY AND TRACK FACILITIES FROM DAMAGE AND WITHOUT INTERRUPTION TO TRAIN OPERATIONS
- (10)

PRIOR TO THE INSTALLATION OF ANY SIGNAGE WITHIN CSXT RIGHT-OF-WAY, CONTRACTORS MUST CONTACT THE RAILROAD’S REPRESENTATIVE FOR LOCATION OF ALL UNDERGROUND SIGNAL UTILITIES.
- (11)

ANY VIOLATION OF ANY CSXT RULES, REGULATIONS OR POLICIES, MAY RESULT IN REMOVAL OF CONTRACTOR PERSONNEL FROM THE RIGHT-OF-WAY.
- (12)

NO CRANE OR BOOM EQUIPMENT SHALL BE ALLOWED TO SET UP TO WORK OR PARK WITHIN BOOM DISTANCE PLUS 15 FEET OF THE CENTERLINE OF TRACK WITHOUT SPECIFIC PERMISSION FROM THE RAILROAD. NO CRANE OR BOOM EQUIPMENT SHALL BE ALLOWED TO FOUL TRACK, WORK WITHIN THE FOUL ZONE, OR LIFT A LOAD OVER THE TRACK WITHOUT FLAGGING PROTECTION AND PERMISSION FOR TRACK TIME FROM THE RAILROAD.
- (13)

ALL WORKMEN AND MACHINE OPERATORS SHALL STAY WITH THEIR MACHINES WHEN CRANE OR BOOM EQUIPMENT IS POINTED TOWARD THE TRACK. ALL CRANES AND BOOM EQUIPMENT SHALL STOP WORK AND CLEAR TRACK WHILE TRAIN IS PASSING. SWINGING LOADS SHALL BE SECURED TO PREVENT MOVEMENT WHILE TRAIN IS PASSING AND NO LOADS SHALL BE SUSPENDED ABOVE A MOVING TRAIN. ALL CRANES AND BOOM EQUIPMENT SHALL BE TURNED AWAY FROM THE TRACK AFTER EACH WORK DAY OR WHENEVER UNATTENDED BY AN OPERATOR.
- (14)

ALL WORK MUST BE STOPPED WHILE TRAINS ARE PASSING WITHIN THE WORK ZONE.

- (15)

“ONE CALL” SERVICES DO NOT LOCATE BURIED RAILROAD SIGNAL AND COMMUNICATIONS LINES. THE CONTRACTOR SHALL CONTACT THE RAILROAD’S REPRESENTATIVE TWO (2) DAYS IN ADVANCE OF THOSE PLACES WHERE EXCAVATION, PILE DRIVING, OR HEAVY LOADS MAY DAMAGE RAILROAD UNDERGROUND LINES ON RAILROAD PROPERTY. UPON REQUEST FROM THE CONTRACTOR OR AGENCY, RAILROAD SIGNAL FORCES WILL LOCATE AND PAINT MARK OR FLAG RAILROAD UNDERGROUND SIGNAL, COMMUNICATION, AND POWER LINES IN THE AREA TO BE DISTURBED FOR THE CONTRACTOR. THE CONTRACTOR SHALL AVOID EXCAVATION OR OTHER DISTURBANCE OF THESE LINES WHICH ARE CRITICAL TO THE SAFETY OF THE RAILROAD AND THE PUBLIC. IF DISTURBANCE OR EXCAVATION IS REQUIRED NEAR A BURIED RAILROAD SIGNAL, COMMUNICATION, OR POWER LINE, THE LINE SHALL BE POTHOLED MANUALLY WITH CAREFUL HAND EXCAVATION BY THE CONTRACTOR AND PROTECTED BY THE CONTRACTOR DURING THE COURSE OF THE DISTURBANCE UNDER THE SUPERVISION AND DIRECTION OF A RAILROAD SIGNAL REPRESENTATIVE.
- (16)

ALL SOILS EXCAVATED WITHIN CSXT’S RAILROAD RIGHT-OF-WAY SHALL REMAIN ON CSXT’S RIGHT-OF-WAY. TESTING OF SOILS ON CSXT ROW IS PROHIBITED WITHOUT PRIOR WRITTEN CSXT AUTHORIZATION. ANY SOILS EXCAVATED ON CSXT ROW CAN BE REUSED ON THE ROW PROVIDED PLACING SOILS ALONG CSXT ROW POSES NO ADVERSE IMPACTS TO THE EXISTING TERRAIN, DRAINAGE OR ENVIRONMENT. SHOULD SOIL NEED TO BE REMOVED FROM CSXT ROW, THE CSXT ENVIRONMENTAL DEPARTMENT WILL SAMPLE THE SOIL FOR DISPOSITION. SOIL STAGED ON CSXT MUST FOLLOW CSXT PROTOCOL AND BE PROPERLY STORED AND/OR PROTECTED FROM THE ELEMENTS AND POTENTIAL EXPOSURE.
- (17)

THE CONTRACTOR SHALL NOTIFY AND COORDINATE THEIR WORK WITH THE FOLLOWING CSXT REPRESENTATIVE:  
  
CSXT GENERAL ENGINEERING CONSULTANT DESIGNATE  
CROUCH ENGINEERING, INC.  
  
5115 MARYLAND WAY, SUITE 225  
BRENTWOOD, TN 37027  
ATTN: MR. SCOTT VICK, P.E. PROJECT DIRECTOR  
PHONE: (615) 791-0630  
EMAIL: SVICK@CROUCHENGINEERING.COM

UTILITY OWNERS

**CABLE:**  
**MEDIA COMM**  
1613 NANTAHALA BEACH ROAD  
GULF BREEZE, FL 32563  
CONTACT: BRANDON JOHANSON  
OFFICE PHONE: 845 570 3292  
CELL PHONE:  
Email: BJOHANSON@MEDIACOMCC.COM

**ELECTRIC:**  
**PULASKI ELECTRIC SYSTEM**  
128 SOUTH 1<sup>ST</sup> STREET  
PULASKI, TN 38478  
CONTACT: CLINT JACKSON  
OFFICE PHONE: 931 363 7052  
CELL PHONE:  
Email: CJACKSON@PESENERGIZE.COM

**GAS:**  
**PULASKI NATURAL GAS**  
203 SOUTH 1<sup>ST</sup> STREET  
PULASKI, TN 38478  
CONTACT: DREW MCMASTERS  
OFFICE PHONE: 931 363 1752  
CELL PHONE:  
Email: PNGD@PULASKI-TN.COM

**TELEPHONE:**  
**AT&T**  
  
116 SOUTH CANNON AVE.  
MURFREESBORO, TN 37129  
CONTACT: KENNETH LEE KORNEGAY  
OFFICE PHONE: 615 848 2082  
CELL PHONE: 615 631 7221  
Email: KK4096@ATT.COM

**WATER:**  
**SOUTH GILES UTILITY DISTRICT**  
8114 ELKTON PIKE  
PROSPECT, TN 38477  
CONTACT: BOBBY PAGE  
OFFICE PHONE: 931 468 2875  
CELL PHONE: 931 638 9246  
Email: BOBBY@SOUTHGILESUD.COM

**TELEPHONE CABLE & FOC:**  
**PULASKI ENERGIZE BROADBAND**  
128 SOUTH 1<sup>ST</sup> STREET  
PULASKI, TN 38478  
CONTACT: GARRET LAARMAN  
OFFICE PHONE: 931 363 2522 EX 7049  
CELL PHONE:  
Email: GLAARMAN@PESENERGIZE.COM

**FOC:**  
**AT&T (FOC)**  
360 GEES MILL BUSINESS PKWY  
CONYERS, GA 30013  
CONTACT: TRINA IVEY  
OFFICE PHONE: 678 641 5522  
CELL PHONE:  
Email: KI2863@ATT.COM

**CABLE:**  
**CHARTER COMMUNICATIONS**  
1850 BUSINESS PARK DR.  
CLARKSVILLE, TN 37040  
CONTACT: DAVID ROBINSON  
OFFICE PHONE: 931 538 1716  
CELL PHONE: 347 804 4063  
Email: DAVID.G.ROBINSON@CHARTER.COM

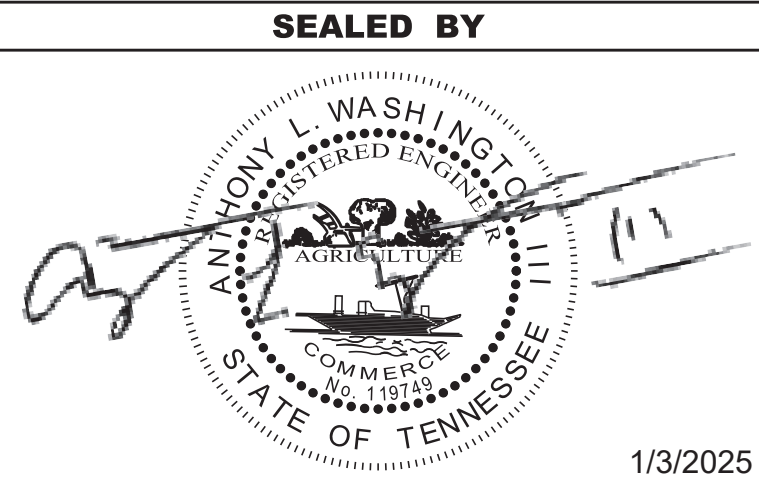
**ELECTRIC:**  
**FAYETTEVILLE ELECTRIC**  
408 COLLEGE STREET WEST  
FAYETTEVILLE, TN 37334  
CONTACT: DANA L. POLLOCK  
OFFICE PHONE: 931 433 1522 EXT. 123  
CELL PHONE: 931 993 1559  
Email: DPOLLOCK@FPU-TN.COM

**ELECTRIC:**  
**TENNESSEE VALLEY AUTHORITY**  
1101 MARKET STREET MR-4G  
CHATTANOOGAI, TN 37402-2801  
CONTACT: STEPHEN WILLIAMS  
OFFICE PHONE: 662 255 6272  
CELL PHONE:  
Email: SEWILLIAMS@TVA.GOV

**TELEPHONE:**  
**WK&T (FORMERLY ARDMORE TELEPHONE COMPANY)**  
100 WK&T TECHNOLOGY DRIVE  
MAYFIELD, KY 42066  
CONTACT: DARREN DUKE  
OFFICE PHONE: 270 856 1877  
CELL PHONE: 270 970 2307  
Email: DDUKE@MYWKT.COOP

**WATER & SEWER:**  
**LINCOLN COUNTY BOARD OF PUBLIC UTILITIES**  
2863 HUNTSVILLE HWY.  
FAYETTEVILLE, TN 37334  
CONTACT: BILLY JOE WILEY  
OFFICE PHONE: 931 433 2259  
CELL PHONE:  
Email: LCBOU@FPUNET.COM

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-15(229)	2C1



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

SPECIAL  
NOTES



ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (4) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (9) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

- (11) SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12) IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

- (15) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

- (1) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

ECOLOGY

- (2) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.
- (3) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.
- (4) ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

PROJECT COMMITMENTS

- (5) SEE PROJECT COMMITMENTS, SHEET 1B, FOR DETAILS RELATING TO SPECIAL ENVIRONMENTAL COMMITMENTS REQUIRED BY THIS PROJECT.

SCOPE OF WORK

- (6) THIS PROJECT INCLUDES MILL, OVERLAY, STRIPING, TRAFFIC CONTROL, AND BROOMING AND DEGRASSING SHOULDERS.

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

- (1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

SEDIMENT CONTROL

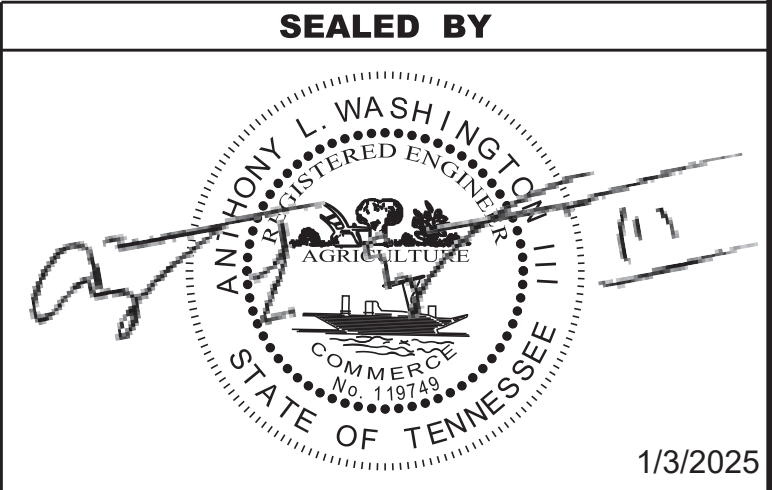
- (6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.

- (35) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (37) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-15(229)	2D



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL  
NOTES



ENVIRONMENTAL NOTES (CONT.)


EPSC GENERAL NOTES CONT.

SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-15(229)	2D1

SEALED BY



1/3/2025

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

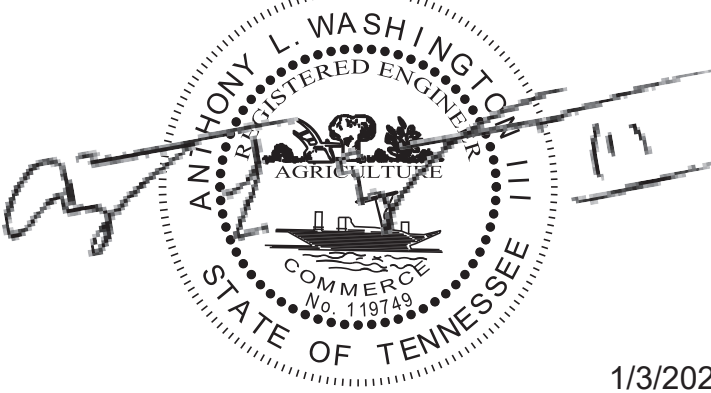
ENVIRONMENTAL  
NOTES



TRAFFIC CONTROL SIGN TABULATION (RESURFACING)					
M.U.T.C.D. SIGN NO.	LEGEND \ DESCRIPTION	SIZE IN INCHES		S.F.	TOTAL NUMBER REQUIRED
		L	x W		ITEM NO. 712-06 S.F.
G20-1	ROAD WORK NEXT 7 MILES	64"	x 24"	11	4
G20-2	END ROAD WORK	48"	x 24"	8	20
W4-2L	LEFT LANE ENDS SYMBOL	48"	x 48"	16	4
W4-2R	RIGHT LANE ENDS SYMBOL	48"	x 48"	16	4
W8-11	UNEVEN LANES	48"	x 48"	16	32
W8-15	GROOVED PAVEMENT	48"	x 48"	16	32
W8-15P	MOTORCYCLE PLAQUE	30"	x 24"	5	32
W20-1	ROAD WORK AHEAD	48"	x 48"	16	16
W20-1	ROAD WORK 1 MILE	48"	x 48"	16	4
W20-1	ROAD WORK 1/2 MILE	48"	x 48"	16	4
W20-1	ROAD WORK 1000 FEET	48"	x 48"	16	4
W20-5L	LEFT LANE CLOSED 1/2 MILE	48"	x 48"	16	4
W20-5L	LEFT LANE CLOSED 1500 FEET	48"	x 48"	16	4
W20-5R	RIGHT LANE CLOSED 1/2 MILE	48"	x 48"	16	4
W20-5R	RIGHT LANE CLOSED 1500 FEET	48"	x 48"	16	4
W21-2	FRESH OIL	48"	x 48"	16	4
W21-5	SHOULDER WORK	48"	x 48"	16	28
				TOTAL	2731

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-15(229)	2E

SEALED BY



1/3/2025

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

TABULATED  
QUANTITIES



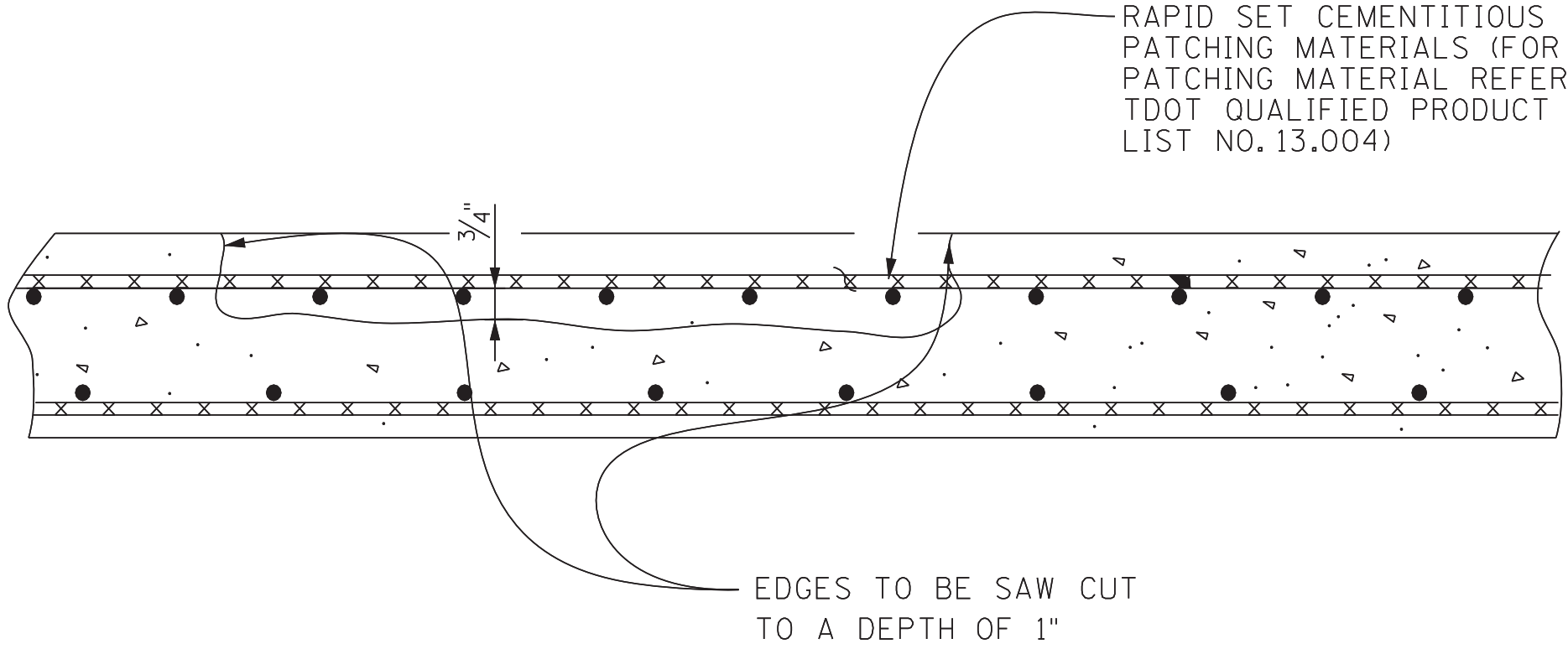
INDEX OF REFERENCE DRAWINGS	DWG. NO.
LAYOUT OF THE BRIDGE	M-339-16
SUPERSTRUCTURE	M-339-19
LAYOUT OF THE BRIDGE	M-258-54
SUPERSTRUCTURE	M-258-57
LAYOUT OF THE BRIDGE	M-258-91
SUPERSTRUCTURE	M-258-98
SUPERSTRUCTURE	M-258-99
REINFORCED CONCRETE PAVEMENT AT THE BRIDGE ENDS	STD-1-5

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
INDEX OF DRAWINGS  
28-SR15-18.39 RT. & LT.  
OVER BUCHANAN CREEK & LANIER RD.  
28-SR-15-23.45  
OVER LITTLE BRADSHAW CREEK  
28-SR15-23.63 RT. & LT.  
BRADSHAW CREEK & CSXT R.R.  
GILES COUNTY  
2025



TABULATION OF BRIDGE RELATED WORK AND ESTIMATED QUANTITIES						
LOCATION OF BRIDGE AND BRIDGE NUMBER	REFERENCE DRAWINGS TO BE PRINTED WITH CONTRACT DRAWINGS	TYPE OF WORK	604-10.44 EXPANSION JOINT REPAIRS L.F.	604-10.50 BRIDGE DECK REPAIRS (PARTIAL DEPTH OF SLAB) S.Y.	604-10.53 CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PAVEMENT) S.Y.	617-04.01 TYPE 1 THIN EPOXY OVERLAY (EPOXY-URETHANE) S.Y.
28-SR15-18.39 RT. OVER BUCHANAN CREEK & LANIER RD. (28SR0150051)	M-339-16 M-339-19 STD-1-5	EXPANSION JOINT REPAIRS BRIDGE DECK REPAIRS TYPE 1 THIN EPOXY OVERLAY	114	5		2682
28-SR15-18.39 LT. OVER BUCHANAN CREEK & LANIER RD. (28SR0150052)	M-339-16 M-339-19 STD-1-5	EXPANSION JOINT REPAIRS BRIDGE DECK REPAIRS TYPE 1 THIN EPOXY OVERLAY	114	5		2682
28-SR15-23.45 OVER LITTLE BRADSHAW CREEK (28SR0150059)	M-258-54 M-258-57	TYPE 1 THIN EPOXY OVERLAY		5		1767
28-SR15-23.63 RT. OVER BRADSHAW CREEK & CSXT R.R. (28SR0150053)	M-258-91 M-258-99 STD-1-5	BRIDGE DECK REPAIRS TYPE 1 THIN EPOXY OVERLAY CONCRETE REPAIR		80	5	4079
28-SR15-23.63 LT. OVER BRADSHAW CREEK & CSXT R.R. (28SR0150054)	M-258-91 M-258-98 STD-1-5	BRIDGE DECK REPAIRS TYPE 1 THIN EPOXY OVERLAY CONCRETE REPAIR		30	5	4416
TOTAL			228	95	10	15626

PROJECT NO.		YEAR	SHEET NO.
28S015-M3-003		2025	B-2
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
- -			
- -			
- -			
- -			
- -			



### SKETCH SHOWING DECK REPAIR

NOTE: PARTIAL DEPTH DECK REPAIR ONLY REMOVE CONCRETE IN ALL DELAMINATED AREAS TO A DEPTH OF ¾" BELOW THE TOP BAR OF THE TOP MAT OF REINFORCING STEEL.

NOTE: PARTIAL DEPTH REPAIRS  
ALL REINFORCING STEEL IN THE AREAS OF THE DECK REPAIRS SHALL BE COMPLETELY CLEANED. CLEANING SHALL BE DONE PRIOR TO PLACING NEW CONCRETE. AREAS OF CONCRETE REMOVAL SHALL BE DESIGNATED BY PERSONNEL FROM THE HEADQUARTERS, BRIDGE INSPECTION AND REPAIR OFFICE. INSPECTIONS TO DETERMINE AREAS OF DECK REPAIR SHALL BE SCHEDULED WITH THE BRIDGE REPAIR OFFICE AT LEAST (3) DAYS IN ADVANCE.

THE COST OF CONCRETE, CONCRETE REMOVAL, LABOR, CLEANING REBAR AND ANY MISCELLANEOUS MATERIAL REQUIRED SHALL BE PAID FOR UNDER ITEM NO. 604-10.50 BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB), S.Y.

NOTE: ITEM NOS. 604-10.50, BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB) SHALL BE BID THIS MAY BE INCREASED, DECREASED OR ELIMINATED AS APPROVED BY THE ENGINEER.

### BRIDGE DECK REPAIR MATERIAL

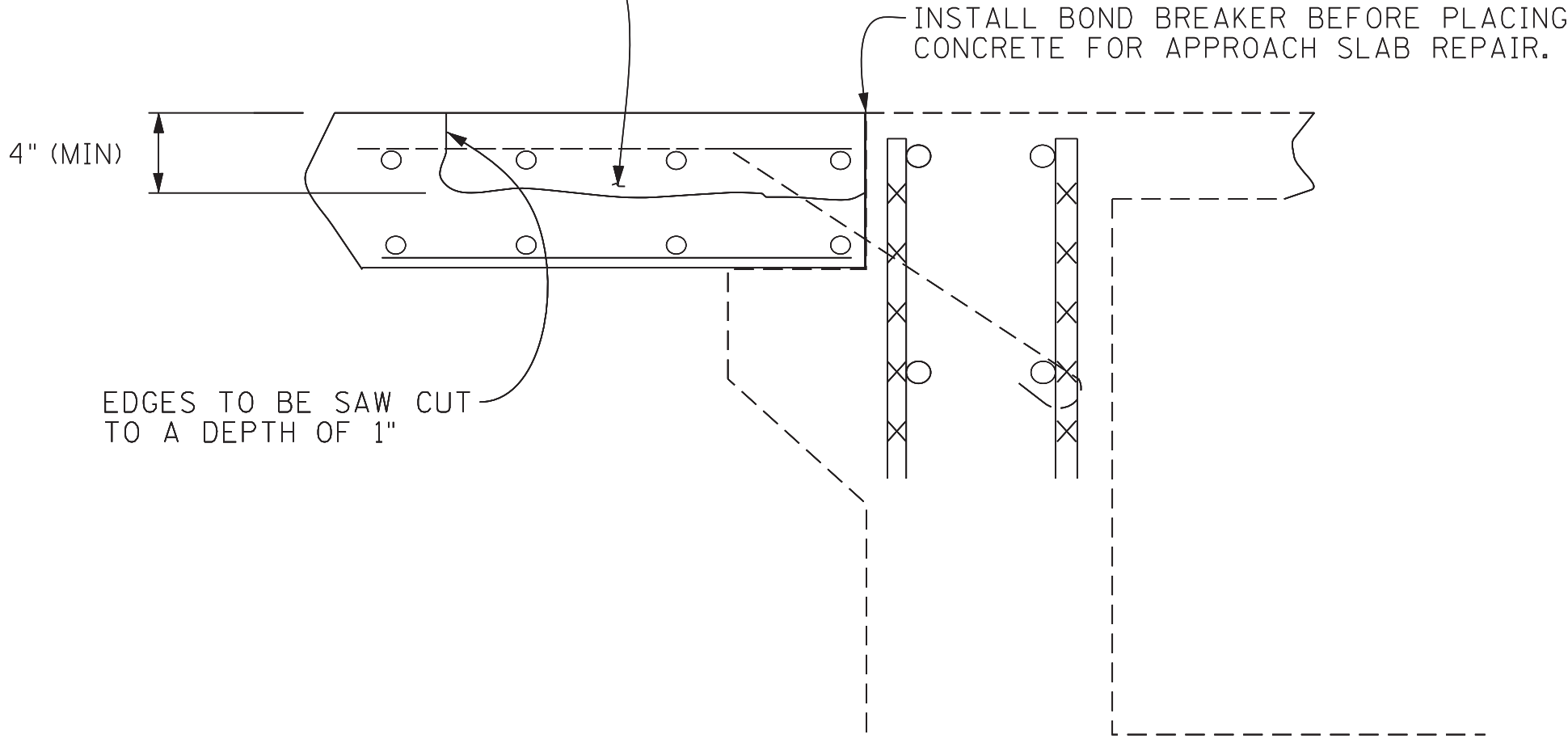
ALL REPAIRS SHALL USE ONLY THE NON-MAGNESIUM PHOSPHATE QUICK SET PATCHING MATERIAL FROM THE TDOT QUALIFIED PRODUCTS LIST: 13.004-RAPID SET CEMENTITIUS PATCHING MATERIALS. SHALL BE APPLIED ACCORDING TO MANUFACTURER'S SPECIFICATIONS.

PATCHING MATERIAL SHOULD BE ABLE TO ACHIEVE A MINIMUM 3000PSI AT 18 HOURS. PATCHING MATERIAL SHALL REACH 3000PSI BEFORE OPENING TO THE TRAFFIC.

### POWER DRIVEN HAND TOOLS:

POWER DRIVEN HAND TOOLS USED FOR THE REMOVAL OF UNSOUND CONCRETE IN MAKING PARTIAL AND FULL DEPTH REPAIRS ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:  
(1) PARTIAL DEPTH REPAIRS; PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 60 LB. CLASS SHALL NOT BE USED. ALSO TRAFFIC CONTROL SHALL BE SET UP DURING PARTIAL DEPTH REPAIRS OVER TRAFFIC.  
(2) FULL DEPTH REPAIRS; PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 90 LB. CLASS SHALL NOT BE USED. ALSO ALL DECK REPAIR OVER BEAMS WILL BE RESTRICTED TO 60 LB. PNEUMATIC HAMMERS.  
(3) CHIPPING HAMMERS OF THE 15 LB. CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL.

SEE BRIDGE DECK REPAIR MATERIAL NOTE BELOW



### CONCRETE APPROACH PAVEMENT REPAIR DETAILS:

NOTE: REMOVE CONCRETE TO A DEPTH OF ¾" MINIMUM BELOW EXISTING REINFORCING STEEL OR TO SOUND CONCRETE. CONCRETE REMOVAL SHOULD BE TO A MINIMUM DEPTH OF 4". DEPTH FOR CONCRETE REMOVAL SHOULD NOT BE MORE THAN 6".

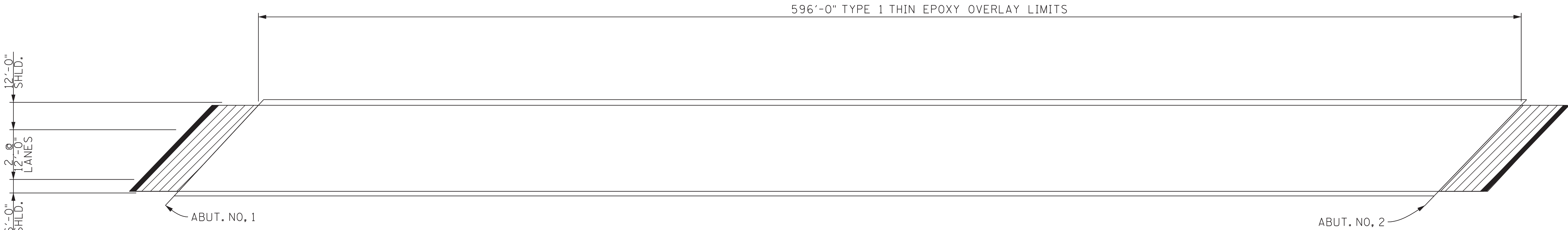
NOTE: COST OF SAW CUTTING, CONCRETE REMOVAL, COMPLETELY CLEANING EXPOSED REINFORCING STEEL, LABOR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO COMPLETE THE REPAIRS AS SHOWN SHALL BE INCLUDED IN ITEM NO. 604-10.53 CONCRETE REPAIR S.Y.

NOTE: ITEM NO. 604-10.53, CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PVMT.) MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.

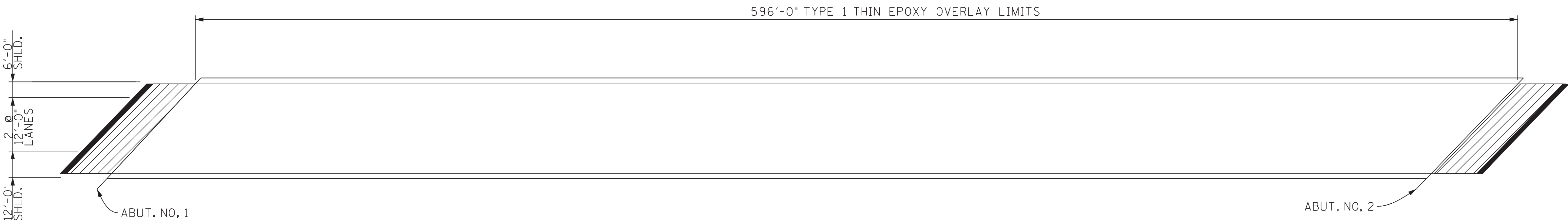
PIN NO.:	115572.00	DATE:	/ /
DESIGN BY:		DATE:	11/24
DRAWN BY:	SILESHI ERGICHO	DATE:	11/24
SUPERVISED BY:	KEVIN MARTINKO	DATE:	/ /
CHECKED BY:		DATE:	/ /

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
BRIDGE TABULATION,  
ESTIMATED QUANTITIES,  
REPAIRS NOTES AND DETAILS  
28-SR015-18.39 RT. & LT.  
OVER BUCHANAN CREEK & LANIER RD.  
28-SR-15-23.45  
OVER LITTLE BRADSHAW CREEK  
28-SR15-23.63 RT. & LT.  
BRADSHAW CREEK & CSXT R.R.  
GILES COUNTY  
2025

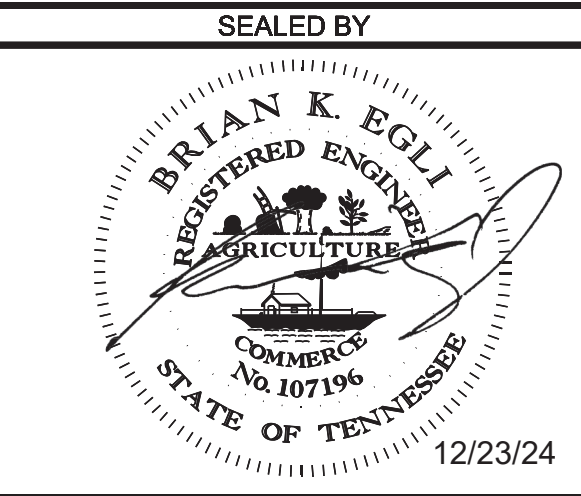
PROJECT NO.		YEAR	SHEET NO.
28S015-M3-003		2025	B-3
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
-	-		
-	-		
-	-		
-	-		
-	-		



PLAN VIEW  
28-SR15-18.39 LT.



PLAN VIEW  
28-SR15-18.39 RT.



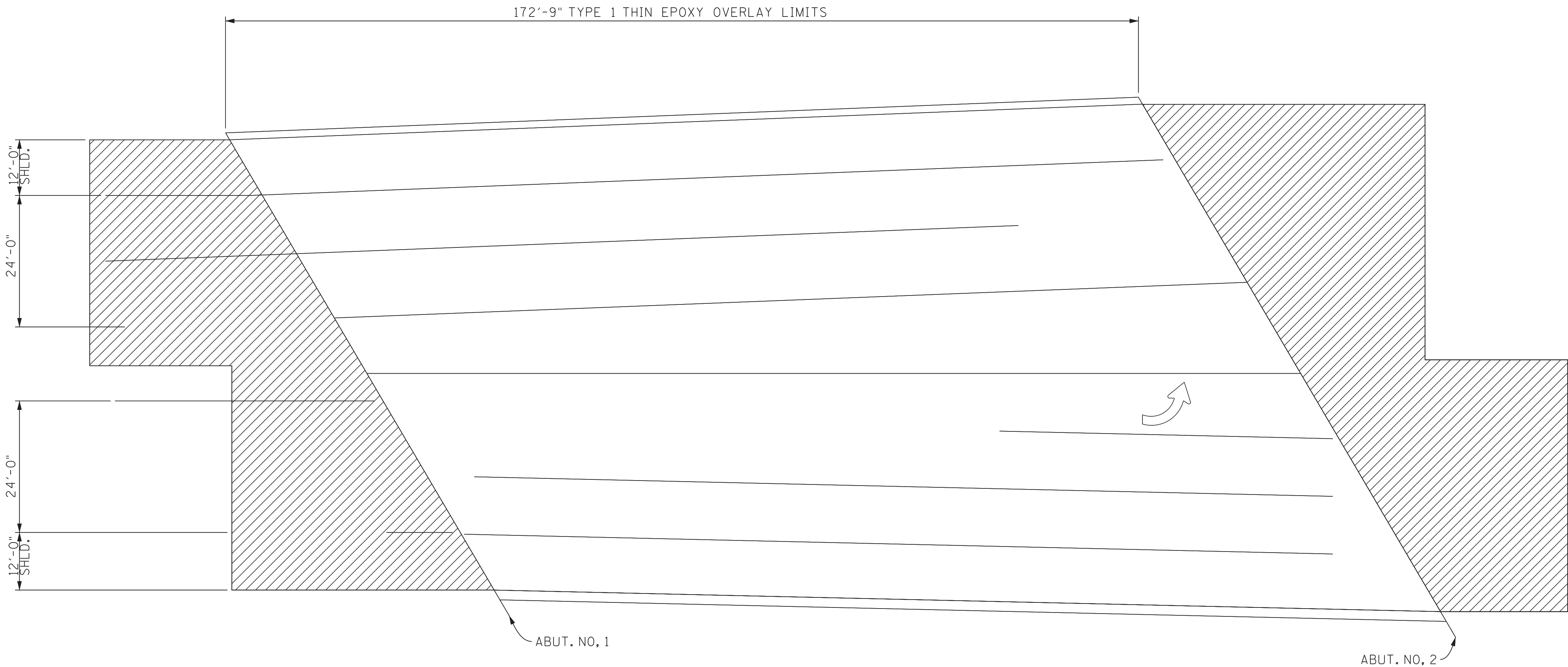
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
PLAN VIEW  
REPAIRS LOCATIONS  
28-SR15-18.39 RT. & LT.  
OVER  
BUCHANAN CREEK & LANIER RD.  
GILES 2025

■ DENOTES: APPROXIMATE JOINT REPAIRS LOCATIONS.  
▨ DENOTES: AREAS THAT HAVE EXISTING ASPHALT OVER  
APPROACH PAVEMENT SHALL BE PAVED, SEE  
PAVING SCHEDULE.

PIN NO.: 115572.00  
DESIGN BY: SILESHI ERGICHO  
DRAWN BY: KEVIN MARTINKO  
SUPERVISED BY: KEVIN MARTINKO  
CHECKED BY:   
DATE: 11/24  
DATE: 11/24  
DATE: 11/24



PROJECT NO.		YEAR	SHEET NO.
28S015-M3-003		2025	B-4
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
	- -		
	- -		
	- -		
	- -		
	- -		

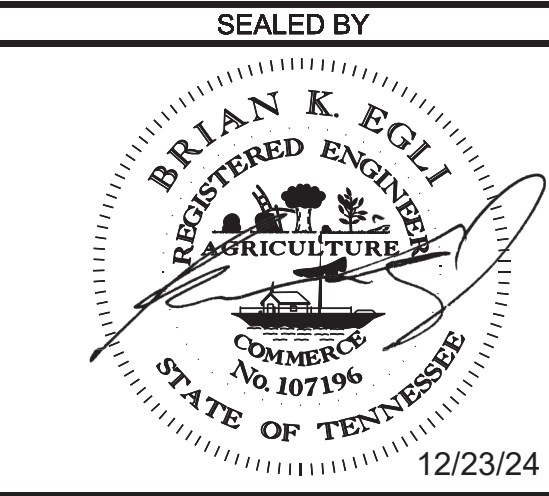


PLAN VIEW  
28-SR15-23.45

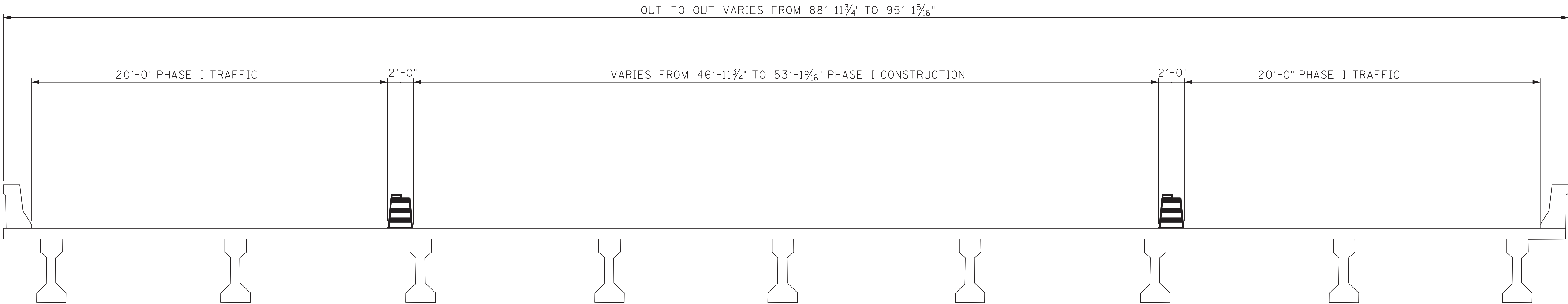
/// DENOTES: AREAS THAT HAVE EXISTING ASPHALT OVER  
APPROACH PAVEMENT SHALL BE PAVED, SEE  
PAVING SCHEDULE.

PIN NO.:	115572.00	DATE:	/ /
DESIGN BY:	SILESHI ERGICHO	DATE:	11/24
DRAWN BY:	KEVIN MARTINKO	DATE:	11/24
SUPERVISED BY:		DATE:	/ /
CHECKED BY:		DATE:	/ /

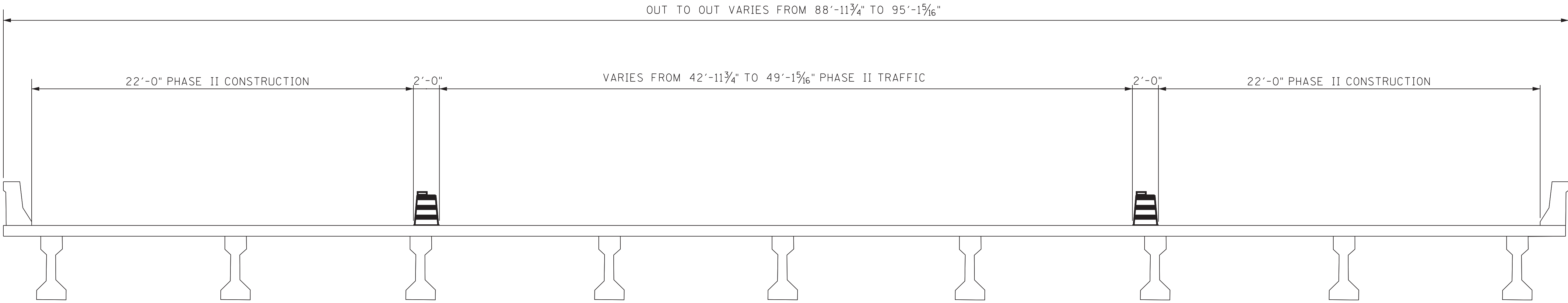
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
PLAN VIEW  
REPAIRS LOCATION  
28-SR15-23.45  
OVER  
LITTLE BRADSHAW CREEK  
GILES 2025



PROJECT NO.		YEAR	SHEET NO.
28S015-M3-003		2025	B-5
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
	--		
	--		
	--		
	--		
	--		



**PHASE I CONSTRUCTION**  
(LOOKING AHEAD ON THE SURVEY)



**PHASE II CONSTRUCTION**  
(LOOKING AHEAD ON THE SURVEY)



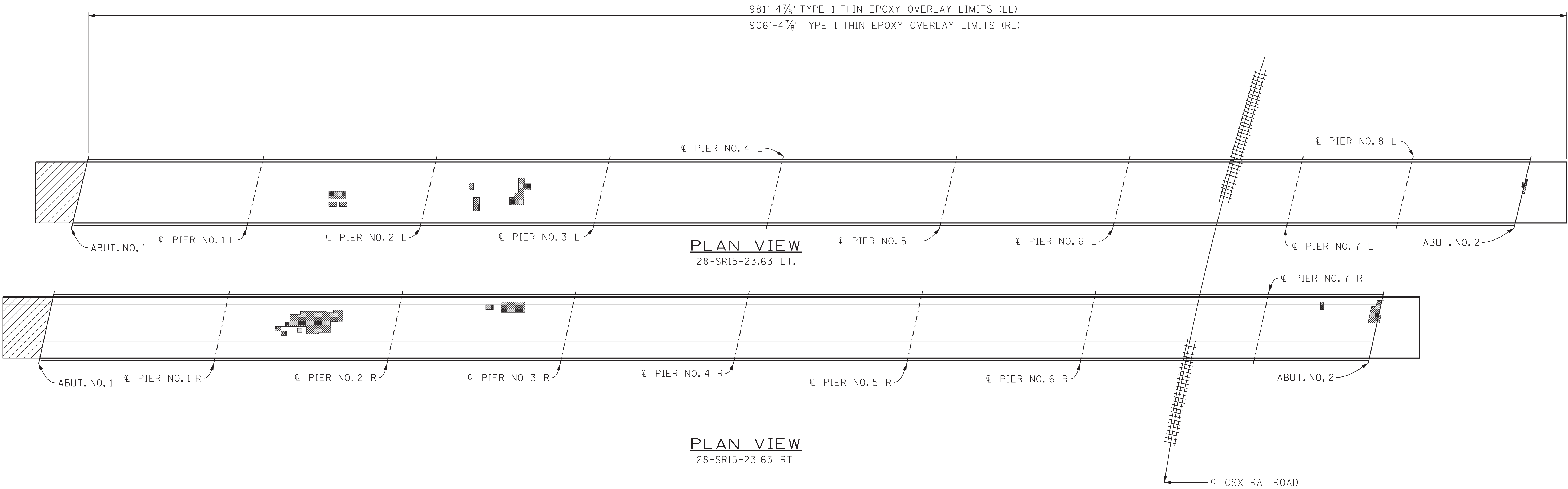
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
PHASE CONSTRUCTION  
28-SR15-23.45  
OVER  
LITTLE BRADSHAW CREEK  
GILES 2025

PIN NO.:	115572.00	DATE:	/ /
DESIGN BY:	SILESHEI ERGICHO	DATE:	11/24
SUPERVISED BY:	KEVIN MARTINKO	DATE:	11/24
CHECKED BY:		DATE:	/ /





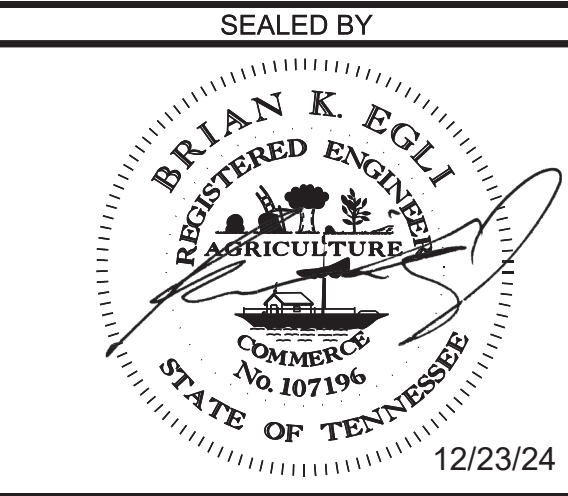
PROJECT NO.		YEAR	SHEET NO.
28S015-M3-003		2025	B-6
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
	- -		
	- -		
	- -		
	- -		
	- -		



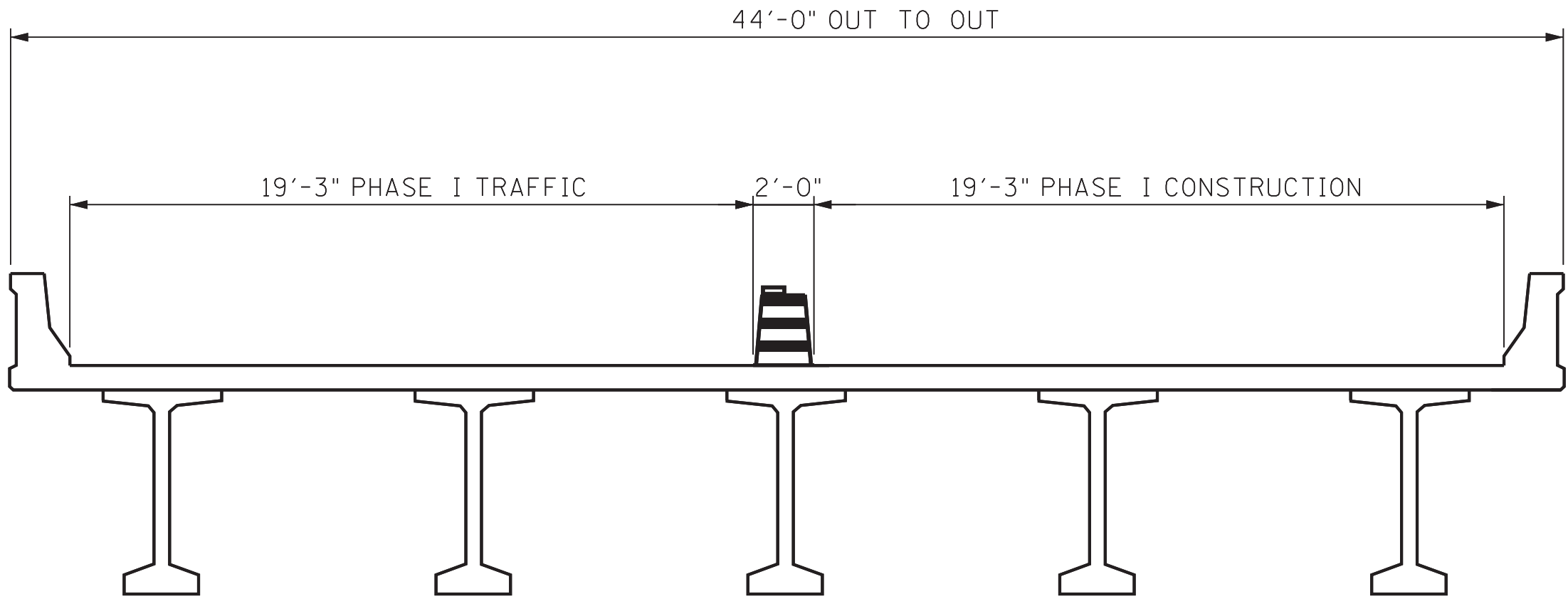
- ▨ DENOTES: AREAS THAT HAVE EXISTING ASPHALT OVER APPROACH PAVEMENT SHALL BE PAVED, SEE PAVING SCHEDULE.
- ▩ DENOTES: APPROXIMATE BRIDGE DECK REPAIRS (PARTIAL DEPTH OF SLAB) LOCATIONS.

PIN NO.:	115572.00
DESIGN BY:	DATE: / /
DRAWN BY: SILESHI ERGICHO	DATE: 11/24
SUPERVISED BY: KEVIN MARTINKO	DATE: 11/24
CHECKED BY:	DATE: / /

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
PLAN VIEW  
REPAIRS LOCATIONS  
28-SR15-23.63 RT. & LT.  
OVER  
BRADSHAW CREEK & CSXT R.R.  
GILES 2025

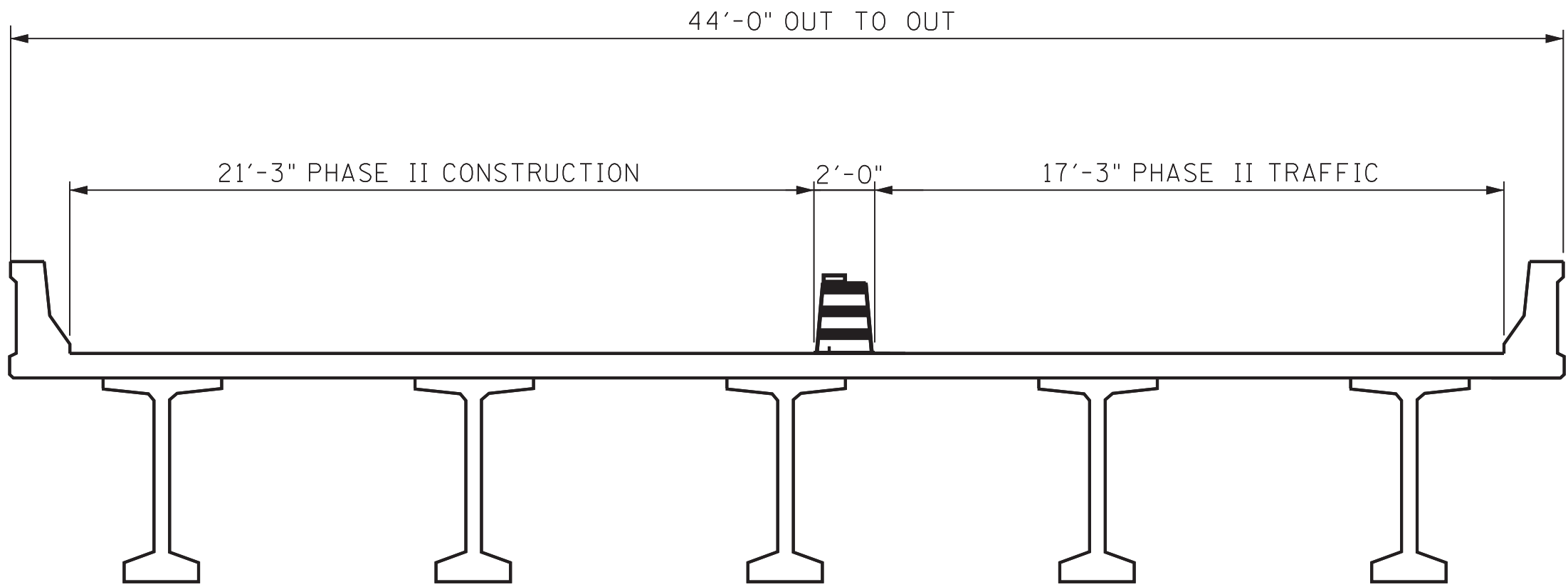


PROJECT NO.		YEAR	SHEET NO.
28S015-M3-003		2025	B-7
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
	--		
	--		
	--		
	--		
	--		



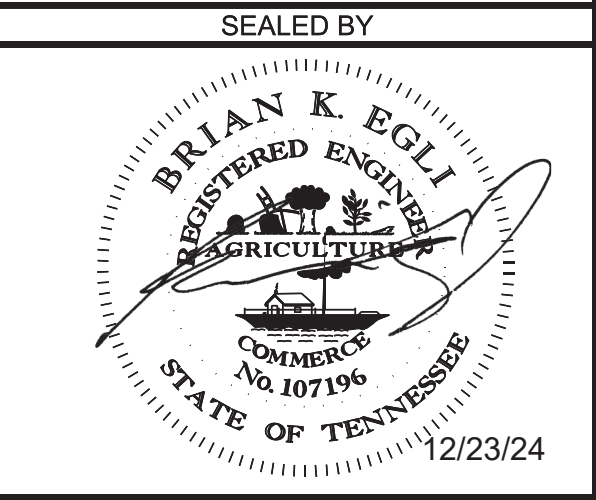
PHASE I CONSTRUCTION

(LOOKING AHEAD ON THE SURVEY)  
(LEFT LANE - LOOKING BACK ON THE SURVEY)  
(RIGHT LANE - LOOKING AHEAD ON THE SURVEY)



PHASE II CONSTRUCTION

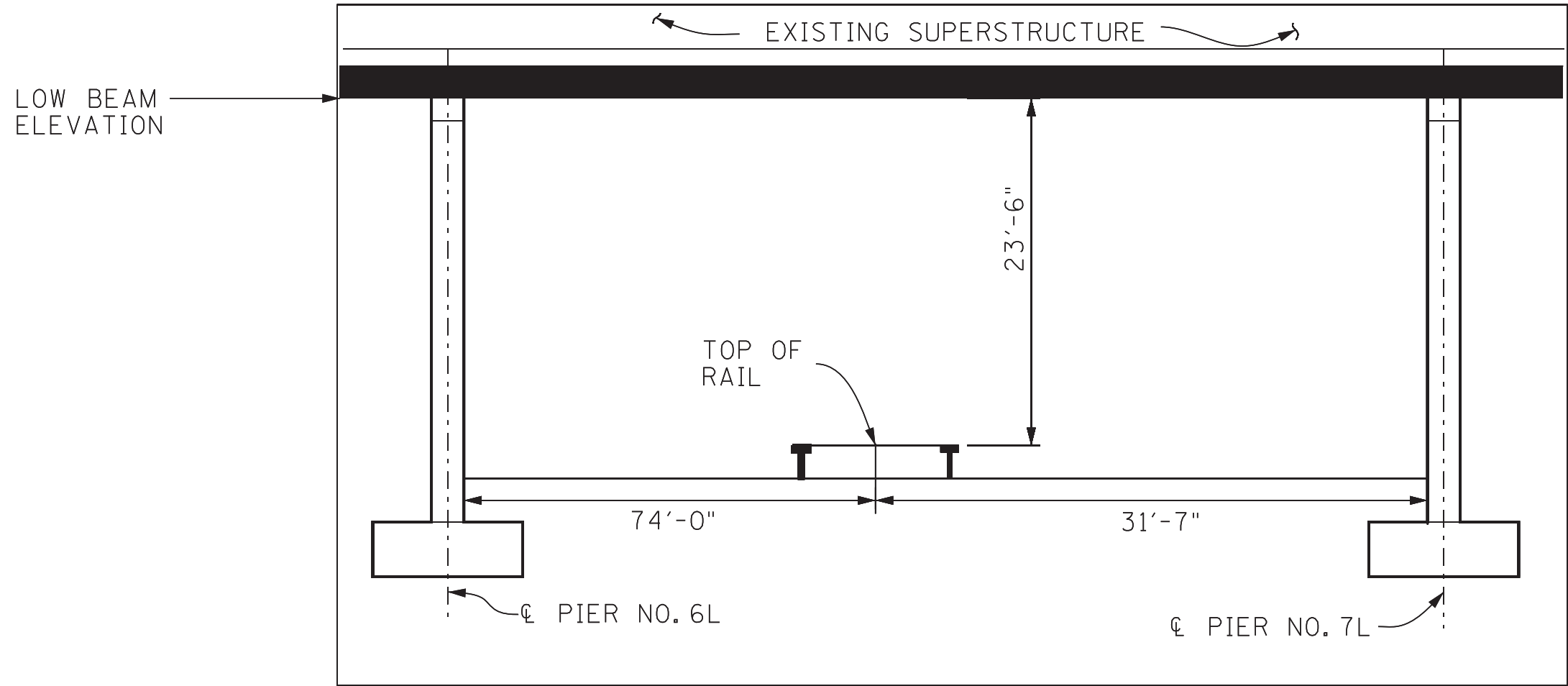
(LOOKING AHEAD ON THE SURVEY)  
(LEFT LANE - LOOKING BACK ON THE SURVEY)  
(RIGHT LANE - LOOKING AHEAD ON THE SURVEY)



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
PHASE CONSTRUCTION  
28-SR15-18.39 RT. & LT. OVER  
BUCHANAN CREEK & LANIER RD.  
AND  
28-SR15-23.63 RT. & LT. OVER  
BRADSHAW CREEK & CSXT R.R.  
GILES 2025

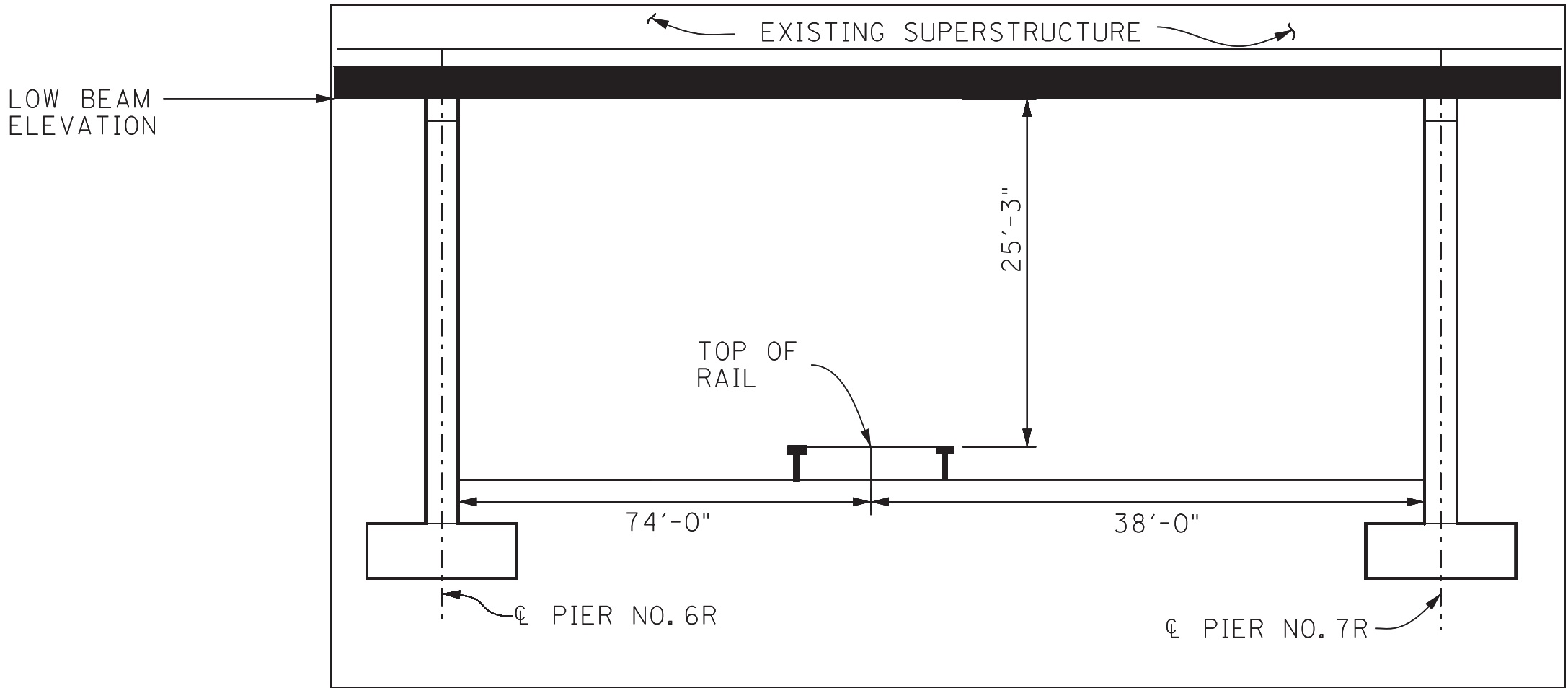
PIN NO.:	115572.00	DATE:	/ /
DESIGN BY:	SILESHI ERGICHO	DATE:	11/24
DRAWN BY:	KEVIN MARTINKO	DATE:	11/24
CHECKED BY:		DATE:	/ /

PROJECT NO.		YEAR	SHEET NO.
28S015-M3-003		2025	B-8
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
	- -		
	- -		
	- -		
	- -		
	- -		



CSXT RAILROAD  
LEFT LANE BRIDGE.

ANY CLEARANCE REDUCTION WILL  
HAVE TO BE APPROVED BY CSXT  
BEFORE PUT IN PLACE.



CSXT RAILROAD  
RIGHT LANE BRIDGE.

### SPECIAL RAILROAD NOTES

- 1.) THE CONTRACTOR SHALL CONDUCT HIS WORK SO AS TO PROTECT THE RAILROAD'S TRACKS AND PROPERTIES FROM ANY DAMAGE. THE WORK SHALL BE DONE IN ACCORDANCE WITH REGULATIONS STIPULATED BY SOUTHERN RAILROAD ON THIS PROJECT SO AS TO MAINTAIN CLEARANCE AND NOT INTERRUPT TRAIN TRAFFIC IN ANY MANNER.
- 2.) THE CONTRACTOR SHALL BE REQUIRED TO SUBMIT A DETAILED AND COMPREHENSIVE SCHEDULE, PLAN AND PROCEDURE FOR REVIEW AND APPROVAL BY CSXT RAILROAD FOR THE FOLLOWING TASKS IN THE RAILROAD'S RIGHT-OF-WAY:
  - A) BRIDGE DECK REPAIRS (PARTIAL DEPTH OF SLAB).
  - B) TYPE 1 THIN EPOXY OVERLAY.
- 3.) NO TEMPORARY FALSEWORK IF APPLICABLE WILL BE ALLOWED THAT INFRINGES ON EXISTING HORIZONTAL AND VERTICAL CLEARANCES (SEE THIS SHEET) WITHOUT PRIOR APPROVAL OF CSXT. IT IS ANTICIPATED THAT FULL TIME FLAGGING WILL ONLY BE REQUIRED FOR FULL DEPTH CONCRETE DECK REPAIRS, AND ANY OTHER FLAGGING SERVICES REQUIREMENTS FOR THE BRIDGE DECK SEAL REPAIR CONSTRUCTION WILL BE AT THE DISCRETION OF THE RAILROAD.
- 4.) SEE RAILROAD AGREEMENT/STATE CONTRACT CURRENT EDITION 105C(R) [RAILROAD SPECIAL PROVISIONS] AND CSXT PUBLIC PROJECTS MANUAL CURRENT EDITION TO SET UP FLAGGING SERVICES, TO SUBMIT FOR INSURANCE REQUIRED FOR WORK ON HIGHWAY BRIDGE OVER THE RAILROAD, AND FOR INFORMATION ON ALL OTHER RAILROAD RULES AND SPECIFICATIONS THAT APPLY TO THIS PROJECT.

### RAILROAD CONTACT ADDRESSES

CSXT GENERAL ENGINEERING CONSULTANT DESIGNATE:

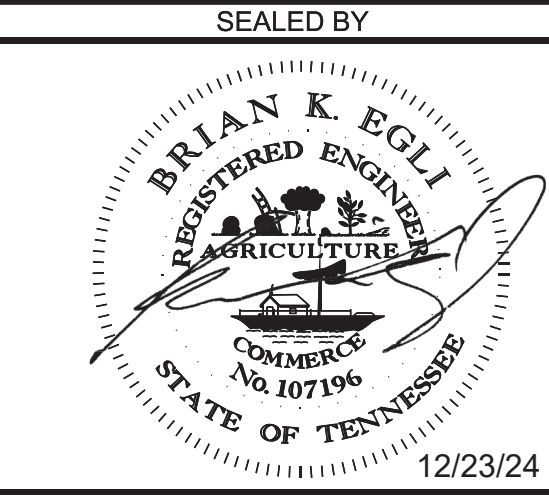
STV, INCORPORATED  
5200 BELFORT RD., SUITE 400  
JACKSONVILLE, FL 32256-6054  
ATTN: MR. RANDY FREDRICK, PROJECT MANAGER

PHONE: (904)383-3913

CELL: (904)254-2692

FAX: (904)730-7766

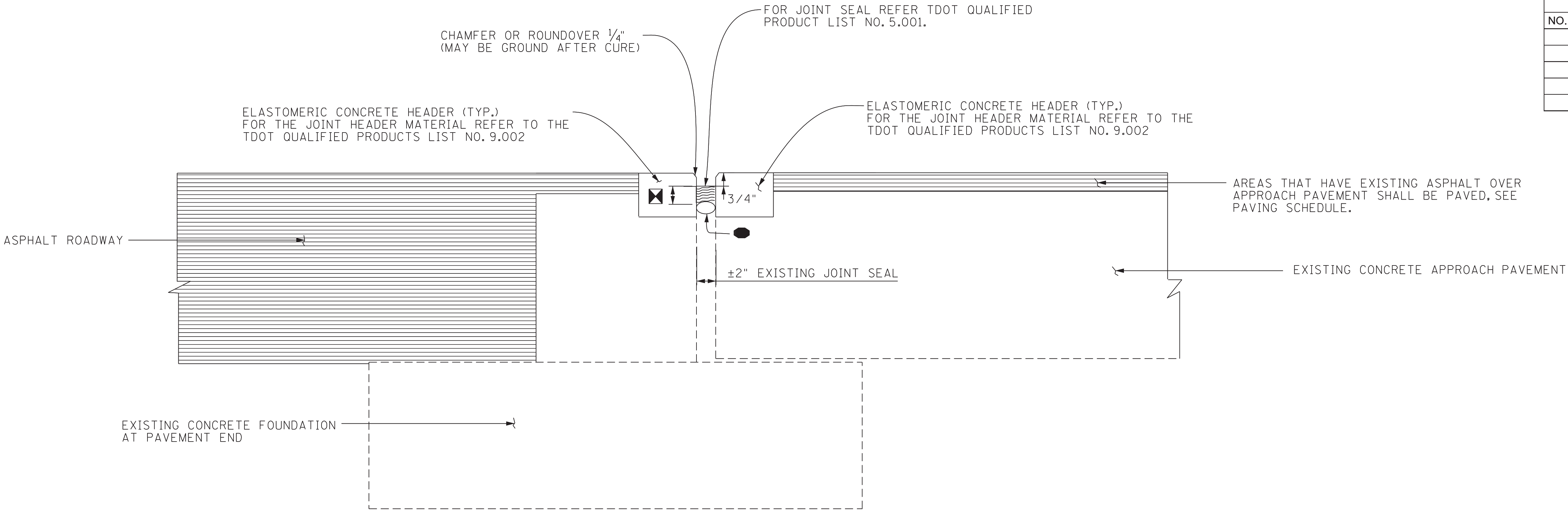
EMAIL: RANDY.FREDERICK@STVINC.COM



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
VERTICAL CLEARANCES AND  
RAILROAD NOTES  
28-SR15-23.63 RT. & LT.  
OVER  
BRADSHAW CREEK & CSXT R.R.  
GILES 2025

PIN NO.: 115572.00  
DESIGN BY: SILESHI ERGICHO  
DRAWN BY: SILESHI ERGICHO  
SUPERVISED BY: KEVIN MARTINKO  
CHECKED BY:   
DATE: / /  
DATE: 11/24  
DATE: 11/24  
DATE: / /

PROJECT NO.		YEAR	SHEET NO.
28S015-M3-003		2025	B-9
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
	- -		
	- -		
	- -		
	- -		
	- -		



EXPANSION JOINT SYSTEM

EXPANSION JOINT REPAIR NOTES:

- NOTE: THE EXISTING JOINT OPENING SHALL BE CAULKED WITH A BACKER ROD OF SUITABLE DIAMETER, THE ROD SHALL BE PLACED AT A DEPTH TO INSURE THE CORRECT WIDTH/DEPTH RATIO OF THE NEW JOINT SEALANT MATERIAL, BACKER ROD SHALL BE AS PER JOINT MANUFACTURER'S RECOMMENDATIONS.
- ☒ NOTE: FULL DEPTH OF ALL EXISTING JOINTS SHALL BE RESEALED WITH NEW JOINT SEALER. THE NEW JOINT SEALER SHALL BE A COLD POUR TWO PART SILICONE SEALER FROM OPL 5.001.

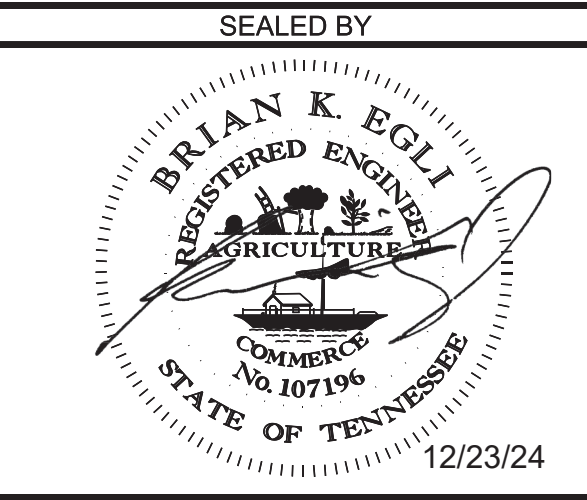
THE JOINT HEADER SYSTEM SHALL BE FROM OPL 9.002. THE JOINT SEAL SYSTEM CONSISTS OF THE ELASTOMERIC HEADER MATERIAL AND A TWO-PART COLD POUR SILICONE SEALANT. THE CONTRACTOR SHALL ALSO HAVE THE OPTION OF USING A PRECOMPRESSED FOAM WITH SILICONE TOPPED PRODUCT (SIMILAR TO AND INCLUDING BEJS) FROM OPL 7.001 WITH AN APPROPRIATELY SIZED SEAL FOR THE JOINT OPENING, AND AN ELASTOMERIC CONCRETE FROM OPL 9.001. THE SYSTEM SHALL BE INSTALLED UNDER THE DIRECT SUPERVISION OF AN AUTHORIZED TECHNICIAN PROVIDED BY THE SYSTEM MANUFACTURER. THE TECHNICIAN MUST APPROVE ALL ASPECTS OF THE GEOMETRY AND PREPARATION OF THE JOINT LOCATIONS PRIOR TO ANY INSTALLATION OF THE JOINT SYSTEM MATERIALS. PRODUCTS FROM OPL 7.001 "ON A ROLL" AND "COMPRESSION ONLY" WILL NOT BE ALLOWED. THE TOP OF THE OPL 7.001 JOINT FILLER SHALL BE A MINIMUM OF 3/4 OF AN INCH BELOW THE ROADWAY SURFACE UNLESS THE MANUFACTURER REQUIRES A DEEPER DEPTH.

MANUFACTURERS SPECIFICATIONS AND INSTALLATION PROCEDURES SHALL BE SUBMITTED TO THE PROJECT ENGINEER FOR REVIEW PRIOR TO THE JOINT REPLACEMENT/REPAIR WORK. THE MANUFACTURER AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE WORKMANSHIP OF THE JOINT INSTALLATION.

AFTER PAVING AND SEALING OVER THE EXISTING EXPANSION JOINT, THE ASPHALT SHALL BE CUT OUT AS SHOWN IN THE NEW EXPANSION JOINT DETAIL.

PRIOR TO THE INSTALLATION OF THE NEW JOINT, THE EXISTING JOINT OPENING SHALL BE CLEANED OF ALL DIRT, DEBRIS, AND PRIOR CONSTRUCTION MATERIAL, ETC., THE FULL DEPTH OF THE OPENING. THE SURFACES WHERE THE NEW MATERIAL BONDS TO STAY IN PLACE, SHALL BE CLEANED PER MANUFACTURERS RECOMMENDATION, TO REMOVE ANY SUBSTANCES THAT WOULD INHIBIT BONDING.

THE COST FOR REMOVING THE OLD JOINT SYSTEM, INSTALLING THE NEW JOINT SYSTEM, LABOR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO INSTALL THE NEW EXPANSION JOINT, IS TO BE INCLUDED UNDER ITEM NUMBER 604-10.44, EXPANSION JOINT REPAIRS, L.F.



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
EXPANSION JOINT  
DETAILS AT APPROACH  
PAVEMENT ENDS  
28-SR15-18.39 RT. & LT.  
OVER  
BUCHANAN CREEK  
& LANIER RD.  
GILES 2025

PIN NO.:	115572.00	DATE:	/ /
DESIGN BY:		DATE:	11/24
DRAWN BY:	SILESHI ERGICHO	DATE:	11/24
SUPERVISED BY:	KEVIN MARTINKO	DATE:	/ /
CHECKED BY:		DATE:	/ /



\$\$\$\$\$TIME\$\$\$\$\$  
\$\$\$\$\$GNSPECS\$\$\$\$\$

PIN NO.:	115572.00	
DESIGN BY:		DATE: / /
DRAWN BY:	SILESHE ERGICHO	DATE: 11/24
SUPERVISED BY:	KEVIN MARTINKO	DATE: 11/24
CHECKED BY:		DATE: / /

TYPE 1 THIN EPOXY OVERLAY NOTES :

TYPE 1 THIN EPOXY OVERLAY SYSTEM - USE DECK PRETREATMENT/PRIMER PER MANUFACTURER'S RECOMMENDATION, AND 2 LIFTS OF AN EPOXY-URETHANE COPOLYMER AND AGGREGATE. TYPE 1 OVERLAY SHALL BE APPLIED MECHANICALLY USING METERED EQUIPMENT; HAND MIXING OF MATERIAL IS NOT PERMITTED.

THIN OVERLAY SYSTEM SHALL BE FROM THE QUALIFIED PRODUCTS LIST 23.005 TYPE 1 THIN OVERLAY (EPOXY URETHANE). MINIMUM OVERLAY THICKNESS SHALL BE 3/8 INCH.

APPLICATION EQUIPMENT SHOULD :

A) BE CAPABLE OF METERING, MIXING AND DISTRIBUTING THE POLYMER AND PRETREATMENT TO MANUFACTURER'S RECOMMENDATION.

B) USE AN APPLICATION MACHINE THAT FEATURES POSITIVE DISPLACEMENT VOLUMETRIC METERING PUMPS CONTROLLED BY A HYDRAULIC POWER UNIT.

C) STORE COMPONENTS IN TEMPERATURE CONTROLLED RESERVOIRS CAPABLE OF MAINTAINING 100 DEGREES FAHRENHEIT (PLUS OR MINUS 10 DEGREES) TO INSURE OPTIMAL MIXING.

D) CHECK MIXING RATIO AT THE PUMP OUTLETS AS WELL AS CYCLE COUNTING CAPABILITIES TO MONITOR OUTPUT ON STANDARD FEATURES.

E) USE MOTIONLESS IN-LINE MIXING SO AS TO NOT OVERLY SHEAR THE MATERIAL TO ENTRAP AIR IN THE MIX.

F) MAXIMIZE MATERIAL WORKING TIME BY MIXING IT IMMEDIATELY BEFORE DISPENSING.

AGGREGATE SHALL BE ANGULAR, HAVING LESS THAN 0.2% MOISTURE AND FREE OF DIRT, CLAY, ASPHALT AND OTHER FOREIGN OR ORGANIC MATERIALS. AGGREGATE FOR ALL LAYERS SHALL BE BAUXITE OR FLINT ROCK PRODUCTS FLINT AND MEETS THE FOLLOWING GRADATION:

SIEVE SIZE	% PASSING
NO. 6	95-100
NO. 10	10-35
NO. 20	0-3

FULL AND PARTIAL DEPTH DECK REPAIR SHALL CURE A MINIMUM OF 28 DAYS BEFORE THE OVERLAY IS PLACED. THE 28 DAYS MAY BE WAIVED IF THE OVERLAY MANUFACTURER PROVIDES A METHOD OF TESTING THE REPAIRED AREAS AND APPROVES THE PLACEMENT BY LETTER. TRAFFIC SHALL BE ALLOWED TO USE THE BRIDGE DURING THE CURING PERIOD OF THE PATCHES BUT NOT AFTER SHOTBLASTING. MAGNESIUM PHOSPHATE BASED MATERIALS WILL NOT BE ALLOWED.

THE CONCRETE DECK SURFACE SHALL BE CLEANED BY SHOTBLASTING TO REMOVE ANY OIL, DIRT, RUBBER, TRAFFIC STRIPING, OR ANY OTHER POTENTIAL DETRIMENTAL MATERIAL SUCH AS CURING COMPOUND AND LAITANCES, WHICH THE MANUFACTURER AND ENGINEER'S OPINION WOULD PREVENT PROPER BONDING AND CURING OF THE MATERIAL. IN AREAS WHERE SHOTBLASTING EQUIPMENT CAN NOT REACH (I.E., ALONG CURBS AND BRIDGE RAILS) SANDBLASTING IS PERMITTED TO AN EXTENT TO THE ENGINEER'S AND MANUFACTURER'S APPROVAL. IMMEDIATELY BEFORE APPLICATION, ALL PREPARED SURFACES SHALL BE CLEANED WITH COMPRESSED AIR OR VACUUMED TO REMOVE DUST AND DEBRIS. THE CONTRACTOR IS TO PREVENT THE TRACKING OF TACK COAT AND CONSTRUCTION DEBRIS ACROSS THE BRIDGE DECK PRIOR TO THE APPLICATION OF THE THIN OVERLAY. MILLING THE BRIDGE DECK WILL NOT BE AN OPTION FOR TACK COAT OR DEBRIS REMOVAL. REMOVAL SHALL BE AT THE CONTRACTOR'S EXPENSE.

ALL SURFACES THAT ARE TREATED SHALL BE DRY AT THE TIME OF APPLICATION. THE OVERLAY SHALL NOT BE APPLIED WHEN IT HAS RAINED 24 HOURS PRIOR TO, OR RAIN IS FORECAST WITHIN 8 HOURS AFTER, APPLICATION. THE MOISTURE CONTENT IN THE DECK SUBSTRATE SHALL BE TESTED. MOISTURE IS NOT TO EXCEED 4.5 PERCENT WHEN MEASURED BY ELECTRONIC METER. IF THE TEST SHOWS EXCESS MOISTURE, THE DECK SHALL CONTINUE TO DRY BEFORE APPLICATION PROCEEDS.

BLUSHING (A WAXY SURFACE COATING ON THE EPOXY) IS CAUSED BY THE REACTION OF MOISTURE WITH THE HARDENING AGENT. BLUSHING CREATES A SURFACE THAT MAKES FUTURE LAYERS DIFFICULT TO ADHERE. LIFTS THAT SHOW SIGNS OF BLUSHING SHALL BE REMOVED AND REPLACED PRIOR TO APPLICATION OF THE NEXT. THE COST TO REMOVE AND REPLACE THESE AREAS SHALL BE AT THE CONTRACTOR'S EXPENSE.

TRAFFIC, OTHER THAN APPLICATION EQUIPMENT, SHALL NOT BE ALLOWED ON ANY PORTION OF THE DECK THAT HAS BEEN SHOTBLASTED OR WHERE PART OF THE APPLICATION HAS BEEN PLACED.

SEE MANUFACTURER'S RECOMMENDATIONS FOR REQUIRED AMBIENT AND SURFACE TEMPERATURES AND HUMIDITY LIMITS FOR APPLICATION.

THE MANUFACTURER SHALL HAVE A REPRESENTATIVE ON THE JOB SITE AT ALL TIMES DURING APPLICATION AND CURE TIME. THE REPRESENTATIVE, ALONG WITH CONSULTATION WITH ENGINEER, MAY SUSPEND ANY ITEM OF WORK THAT IS SUSPECT AND DOES NOT MEET THE REQUIREMENTS OF THE SPECIFICATIONS. WORK SHALL NOT RESUME UNTIL THE ENGINEER AND REPRESENTATIVE ARE SATISFIED THAT APPROPRIATE REMEDIAL ACTION HAS BEEN TAKEN BY THE CONTRACTOR.

ALL COSTS FOR AGGREGATE, EPOXY FOR MINIMUM OF TWO LIFTS, SURFACE PREPARATION, LABOR AND ANY OTHER MISCELLANEOUS MATERIALS REQUIRED TO PLACE THIN OVERLAY SHALL BE INCLUDED IN ITEM NO. 617-04.01, TYPE 1 THIN EPOXY OVERLAY (EPOXY URETHANE), S.Y.

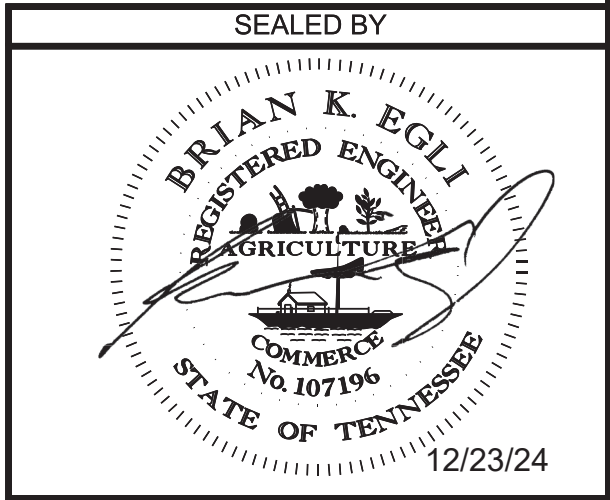
THICKNESS VERIFICATION: THE PROJECT ENGINEER SHALL BE NOTIFIED OF THE NUMBER OF GALLONS USED ON THE PROJECT WITH NOTARIZED QUANTITY STATEMENTS FROM THE CONTRACTOR AND THE MANUFACTURER. THE CONTRACTOR SHALL VERIFY TO TDOT THAT THE OVERLAY IS AN AVERAGE OF AT LEAST 3/8 INCH THICK AT THREE RANDOM LOCATIONS AGREED UPON BY THE PROJECT ENGINEER AND THE MATERIAL MANUFACTURER REPRESENTATIVE. IF 3/8 INCH AVERAGE IS NOT ACHIEVED, A RETEST SHALL BE PERFORMED IN ADJOINING AREAS. THIN AREAS SHALL BE RE-COATED AS DESCRIBED ABOVE BY THE CONTRACTOR AND RE-VERIFIED AT NO ADDITIONAL COST TO TDOT. THIS VERIFICATION MAY CONSIST OF CORES MADE BY THE CONTRACTOR WITH A CORING BIT NOT LESS THAN 1½" DIAMETER. THE TESTED AREAS SHALL BE REPAIRED BY THE CONTRACTOR BEFORE FINAL ACCEPTANCE BY THE PROJECT ENGINEER.

THE MANUFACTURER SHALL HAVE A REP ON THE JOB SITE AT ALL TIME DURING APPLICATION AND CURE TIME. THE REP WITH THE ENGINEER, MAY SUSPEND AN ITEM OF WORK THAT IS SUSPECT AND DOES NOT MEET THE REQUIREMENTS OF THE SPECS. WORK SHALL NOT RESUME UNTIL THE ENGINEER AND REP ARE SATISFIED THAT APPROPRIATE REMEDIAL ACTION HAS BEEN TAKEN BY THE CONTRACTOR.

PROJECT NO.		YEAR		SHEET NO.	
28S015-M3-003		2025		B-10	
REVISIONS					
NO.	DATE	BY	BRIEF DESCRIPTION		
	- -				
	- -				
	- -				
	- -				
	- -				

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
TYPE 1 THIN EPOXY  
OVERLAY NOTES  
28-SR015-18.39 RT. & LT.  
OVER BUCHANAN CREEK & LANIER RD.  
28-SR-15-23.45  
OVER LITTLE BRADSHAW CREEK  
28-SR15-23.63 RT. & LT.  
BRADSHAW CREEK & CSXT R.R.  
GILES COUNTY  
2025

B-10





DRAINAGE AREA = 9.45 SQ. MILES.  
DESIGN DISCHARGE (100 YR.) = 5640 cfs.  
WATER AREA PROVIDED BELOW EL. 687.30 = 1014 SQ. FT.  
100 YEAR VELOCITY = 5.56 FT./SEC.  
100 YR. BRIDGE BACKWATER = 0.92 FT.  
100 YEAR DISCHARGE = 5640 cfs AT ELEV. 688.22.  
500 YEAR DISCHARGE = 7390 cfs AT ELEV. 689.12 .  
OVERTOPPING ELEV. = 703.06

[illegible]

TITLE	DWG. NO.	LATEST REV. DATE
LAYOUT OF BRIDGE NO. 2	M-339-16	
GENERAL NOTES & ESTIMATED QUANTITIES	M-339-17	
FOUNDATION DATA	M-339-18	
SUPERSTRUCTURE	M-339-19	
SUPERSTRUCTURE DETAILS	M-339-20	
PRESTRESSED BULB-T BEAM DETAILS	M-339-21	
ABUTMENT NO. 1 (LEFT LANE)	M-339-22	
ABUTMENT NO. 2 (LEFT LANE)	M-339-23	
ABUTMENT NO. 1 (RIGHT LANE)	M-339-24	
ABUTMENT NO. 2 (RIGHT LANE)	M-339-25	
ABUTMENT NO. 1 & 2 DETAILS	M-339-26	
BENT NO. 1 THRU 4 (LEFT LANE)	M-339-27	
BENT NO. 1 THRU 4 (RIGHT LANE)	M-339-28	
BENT DETAILS	M-339-29	
FINAL FOUNDATION DATA	M-339-30	
BILL OF STEEL (LEFT LANE)	M-339-31	
BILL OF STEEL (RIGHT LANE)	M-339-32	

TITLE	DWG. NO.	DATE
BRIDGE RAILING CONCRETE PARAPET-----	STD-1-1-----	12-18-95
SLIDER PLATES AND DECK DRAINS-----	STD-1-2-----	09-11-95
PAVEMENT AT BRIDGE ENDS-----	STD-1-5-----	12-18-95
BRIDGE END DRAINS W/ PAVEMENT AT BRIDGE ENDS-----	STD-1-6-----	05-01-95
BRIDGE END DRAINS W/ PAVEMENT AT BRIDGE ENDS-----	STD-1-7-----	05-01-95
BRIDGE END DRAINS 4' X 8'-7" W/ PAVEMENT AT BRIDGE ENDS-----	STD-1-9-----	05-01-95
STANDARD PRECAST, PRESTRESSED BRIDGE DECK PANELS		
GENERAL DETAILS-----	STD-4-1-----	06-10-96
STANDARD PRECAST, PRESTRESSED BRIDGE DECK PANELS		
DESIGN CRITERIA-----	STD-4-2-----	06-10-96
STANDARD PRECAST, PRESTRESSED BRIDGE DECK PANELS		
GENERAL DETAILS-----	STD-4-3-----	06-10-96
STANDARD PRECAST, PRESTRESSED BRIDGE DECK PANELS		
CONSTRUCTION DETAILS-----	STD-4-4-----	06-10-96
STANDARD PILE DETAILS-----	STD-5-1-----	10-25-93
STANDARD PILE DETAILS-----	STD-5-2-----	10-26-92
STANDARD SEISMIC DETAILS-----	STD-6-1-----	05-01-95
REINFORCING BAR SUPPORT DETAILS		
FOR CONCRETE SLAB-----	STD-9-1-----	12-19-94
MISCELLANEOUS ABUTMENT AND DRAINAGE DETAILS-----	STD-10-1-----	05-11-92
STANDARD DETAILS AND INTERMEDIATE DIAPHRAGM		
DETAILS FOR BULB-TEE BEAMS-----	STD-14-1-----	02-14-95
SLOPE PROTECTION-----	RD-SA-1-----	05-27-96

REGARDING	PROV. NO.	LATEST REV. DATE
APPROVAL OF SHOP DRAWINGS -----	105A -----	03-06-95

2016 ADT = 4970  
TWIN 42'-0" ROADWAYS W/ STD-1-1 PARAPET  
DESIGN SPEED = 60 MPH

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
LAYOUT OF BRIDGE NO. 2  
STATE ROUTE 15  
OVER  
BUCHANAN CREEK  
BRIDGE I.D. NO. 28SR0150051 (RL)  
BRIDGE I.D. NO. 28SR0150052 (LL)  
STATION 503+80.63  
GILES COUNTY  
1996

CORRECT Edward P. Wasserman  
ENGINEER OF STRUCTURES

M-339-16

DESIGNED BY T. TOLLEY DATE 5-96  
DRAWN BY BUTCH LOWERY DATE 5-96  
SUPERVISED BY R.L.H. \ DATE 5-96  
CHECKED BY T. TOLLEY DATE 10-96

CURVE DATA  
(PRIVATE DRIVE)

(PRIVATE DRIVE)

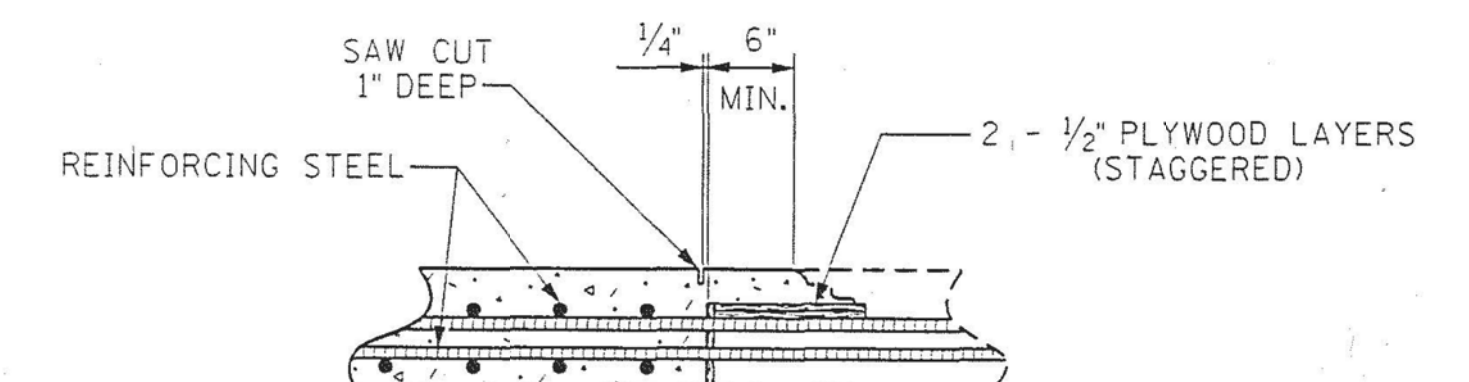
C1	C2	C3	C4
P.I. = 0+56.73	P.I. = 1+67.83	P.I. = 4+57.65	P.I. = 6+39.14
N = 309,225.00	N = 309,325.22	N = 309,601.131	N = 309,735.50
E = 1,686,912.00	E = 1,686,960.712	E = 1,687,050.006	E = 1,686,895.50
$\Delta = 10^{\circ}50'25"$ RT.	$\Delta = 7^{\circ}59'19"$ LT.	$\Delta = 66^{\circ}55'15"$ LT	$\Delta = 18^{\circ}40'33"$ RT
T = 55.71'	T = 55.71'	T = 100'	T = 80'
R = 587.15'	R = 797.83'	R = 151.31'	R = 248.61'
Dc = 9'45"30"	Dc = 7'10"53"	Dc = 3'7"51"59"	Dc = 23'02"47"
Lc = 111.09'	Lc = 111.24'	Lc = 176.73'	Lc = 154.80'
Ex = 2.64'	Ex = 1.94'	Ex = 30.06'	Ex = 12.55'
P.C. = 0+01.02	P.C. = 1+12.12	P.C. = 3+57.65	P.C. = 5+59.14
P.T. = 1+12.11	P.T. = 2+23.36	P.T. = 4+34.38	P.T. = 7+13.94

P.T. STA. 497+25  
[E] 770 71

LIMITS OF  
STRUCTURE

GRADE SKETCH





SLAB CONSTRUCTION JOINT DETAIL

DECK CONCRETE POURING SEQUENCE: SLAB CONSTRUCTION JOINTS MAY BE LOCATED AT THE CONTRACTOR'S OPTION SUBJECT TO THE FOLLOWING:

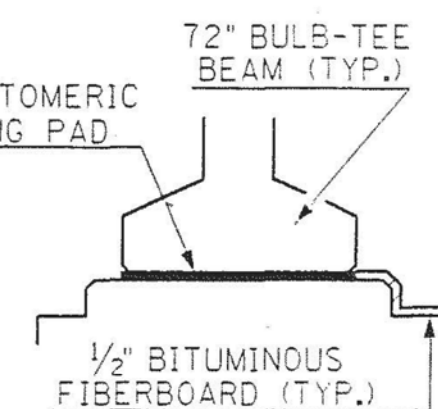
- 1) NO CONSTRUCTION JOINT MAY BE LOCATED CLOSER THAN 10 FEET OR FURTHER THAN 15 FEET FROM AN INTERIOR SUPPORT.
- 2) THE SLAB IN THE MIDDLE SECTION OF BOTH ADJACENT SPANS MUST BE POURED TO WITHIN AT LEAST 15 FEET OF THE SUPPORTS EITHER PRIOR TO OR CONCURRENTLY WITH THE SLAB OVER AN INTERIOR SUPPORT.

ALL SLAB CONSTRUCTION JOINTS SHALL BE IN ACCORDANCE WITH THE "SLAB CONSTRUCTION JOINT DETAIL" SHOWN ABOVE.

NOTE: NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.

NOTE: THE CONTRACTOR IS SOLELY RESPONSIBLE FOR SUPPORTING THE BEAMS TO PREVENT DAMAGE DUE TO TWISTING OR OVERTURNING DURING ALL PHASES OF CONSTRUCTION. IT IS STRONGLY RECOMMENDED THAT THE TEMPORARY ERECTION DIAPHRAGMS BE INSTALLED IN THE SPAN AND THE INTERMEDIATE DIAPHRAGMS BE POURED AND CURED PRIOR TO PLACING ANY LOADS ON THE GIRDERS. HOWEVER, TEMPORARY ERECTION DIAPHRAGMS AND PERMANENT INTERMEDIATE DIAPHRAGMS MUST BE IN PLACE IN THE SPAN AT THE TIME THE SLAB IS POURED IN SAID SPAN.

NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED. ALSO SEE DRAWING NO. STD-1-1.

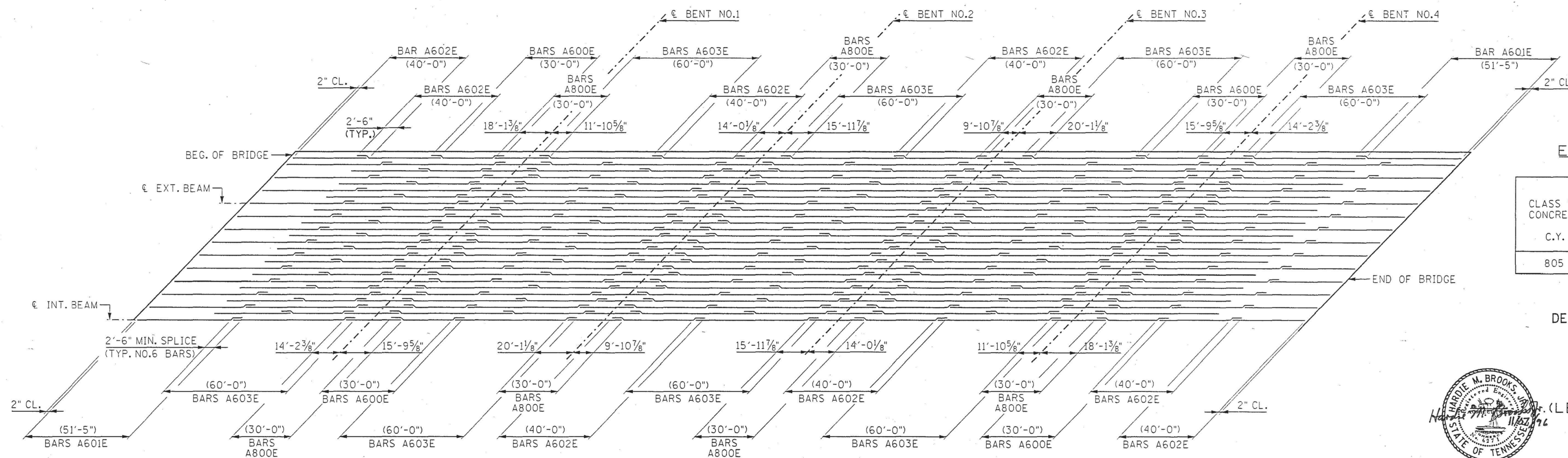


DETAIL "A"

TYPICAL AT SUPPORT

TYPICAL AT MID-SPAN  
(SPAN NO.1 & 5 ONLY)

TYPICAL CROSS SECTION  
(LOOKING FORWARD ON SURVEY)



PART-PLAN OF MAIN REINFORCING

ESTIMATED QUANTITIES  
(EACH LANE)

CLASS "D" CONCRETE	EPOXY COATED STEEL REINFORCING	STEEL BAR REINFORCING
C.Y.	LB.	LB.
805	230,132	6,381

BRIDGE NO.2

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

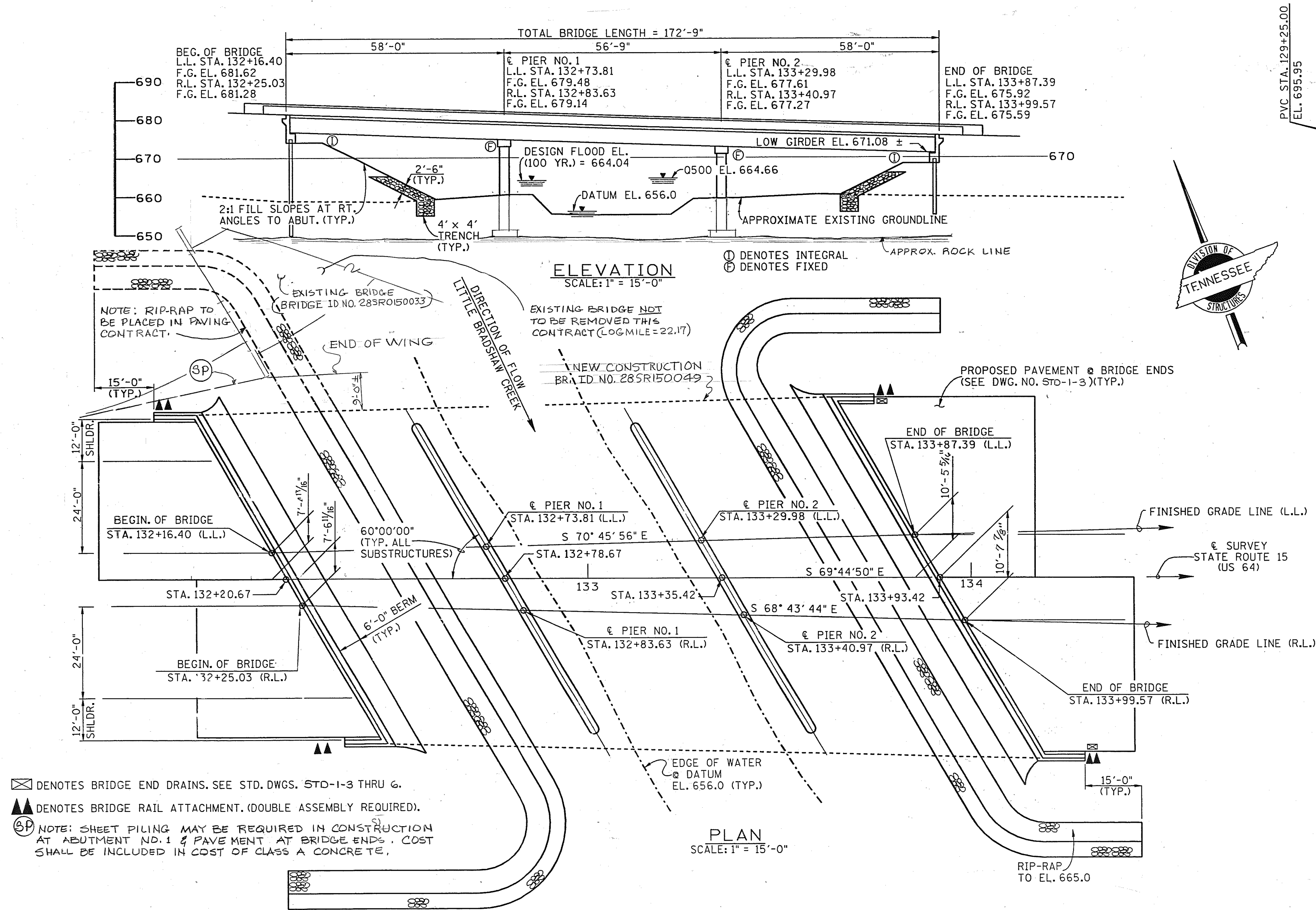
SUPERSTRUCTURE  
STATE ROUTE 15  
OVER  
BUCHANAN CREEK  
(LEFT AND RIGHT LANES)  
STATION 503+80.63  
GILES COUNTY  
1996



CORRECT Edward P. Wasserman  
ENGINEER OF STRUCTURES

DESIGNED BY T. TOLLEY DATE 3-96  
DRAWN BY ANDREW L. CHESSE DATE 6-96  
SUPERVISED BY R.L.H. / J.W.P. DATE 6-96  
CHECKED BY T. TOLLEY DATE 10-96





⊠ DENOTES BRIDGE END DRAINS. SEE STD. DWGS. STD-1-3 THRU 6.

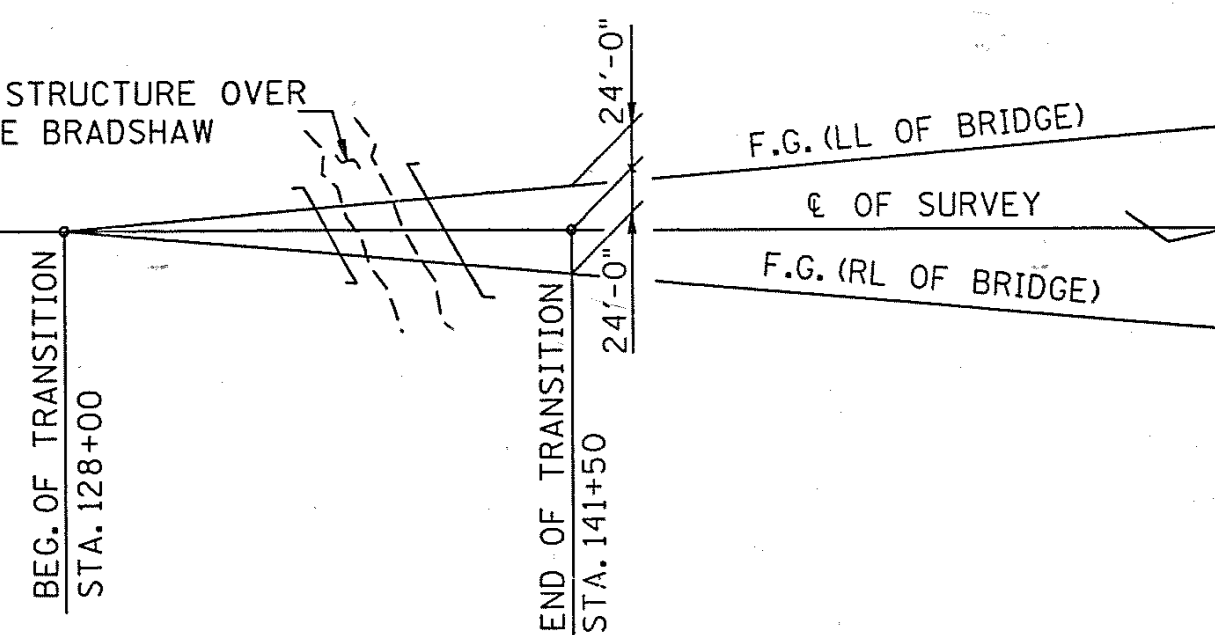
▲▲ DENOTES BRIDGE RAIL ATTACHMENT. (DOUBLE ASSEMBLY REQUIRED).

Ⓢ NOTE: SHEET PILING MAY BE REQUIRED IN CONSTRUCTION AT ABUTMENT NO. 1 & PAVEMENT AT BRIDGE ENDS. COST SHALL BE INCLUDED IN COST OF CLASS A CONCRETE.

### HYDRAULIC DATA

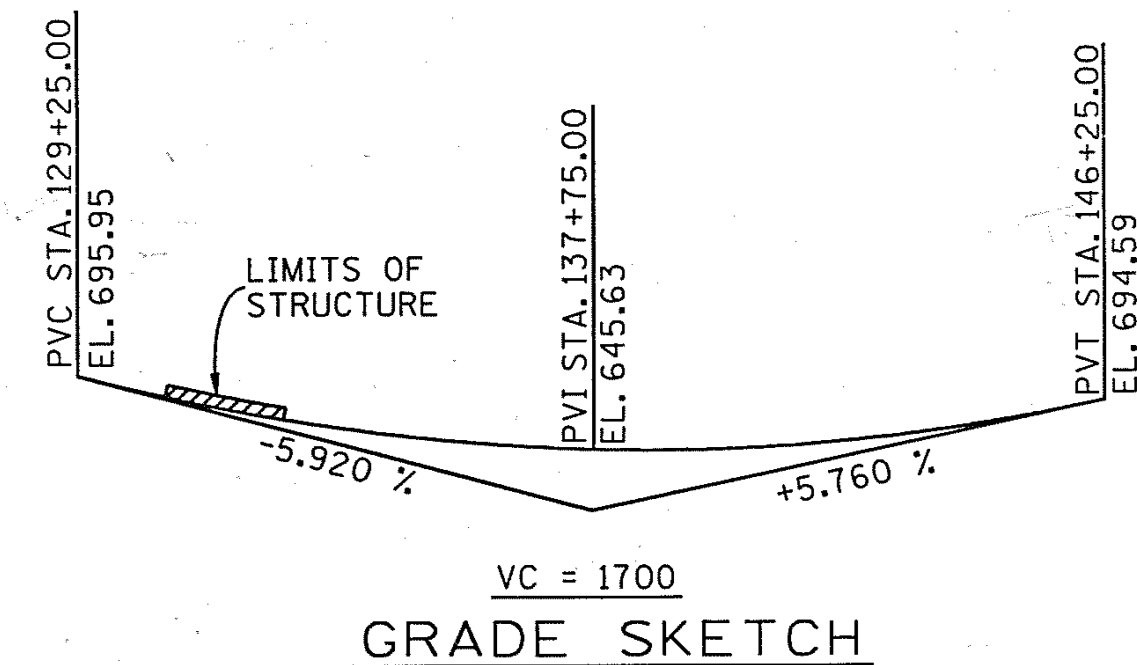
DRAINAGE AREA = 4.3 SQ. MILES  
 DESIGN DISCHARGE (100 YR.) = 3210 cfs  
 WATER AREA PROVIDED BELOW EL. 664.0 = 477 SQ. FT.  
 100 YEAR VELOCITY = 6.7 FT./SEC.  
 100 YR. BRIDGE BACKWATER = 1.0 FT.  
 ROADWAY OVERTOPPING ELEV. = 673.5  
 500 YEAR DISCHARGE = 4180 cfs AT ELEV. 664.66

PROPOSED STRUCTURE OVER  
 LITTLE BRADSHAW



SKETCH SHOWING MEDIAN VARIATION

DESIGNED BY WEHBI & CRAWFORD DATE 3-90  
 DRAWN BY GEORGE KORNICKOSKI DATE 5-90  
 SUPERVISED BY LKB & SMH & HMB DATE 5-90  
 CHECKED BY DATE



CONST. NO. 28007-3220-04

PROJECT NO.	YEAR	SHEET NO.
SP	1991	

### REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION
1	3-30-92	PS	ADDED SHEET M-258-58A TO LIST OF DRAWINGS

### LIST OF DRAWINGS

DWG. NO.	LAST REV. DATE
LAYOUT OF BRIDGE	M-258-54
GENERAL NOTES AND ESTIMATED QUANTITIES	M-258-55
FOUNDATION DATA	M-258-56
SUPERSTRUCTURE	M-258-57
SUPERSTRUCTURE DETAILS	M-258-58
SUPERSTRUCTURE DETAILS	M-258-59
SUPERSTRUCTURE DETAILS	M-258-60
PRESTRESSED I-BEAMS	M-258-61
ABUTMENT NO.1	M-258-62
ABUTMENT NO.1 DETAILS	M-258-63
ABUTMENT NO.2	M-258-64
ABUTMENT NO.2 DETAILS	M-258-65
PIERS NO.1	M-258-66
PIERS NO.2	M-258-67
BILL OF STEEL	M-258-68
BILL OF STEEL	M-258-69
BRIDGE SCREENS	M-258-58A

### LIST OF STD. DRAWINGS

DWG. NO.	LAST REV. DATE
REINFORCING BAR SUPPORT DETAILS FOR CONCRETE SLAB	STD-9-1
MISCELLANEOUS ABUTMENT AND DRAINAGE DETAILS	STD-10-1
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS DESIGN CRITERIA	STD-4-1
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	STD-4-2
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS CONSTRUCTION DETAILS	STD-4-3
STD. PILE DETAILS	STD-5-1
STD. PILE DETAILS	STD-5-2
BRIDGE RAILING CONCRETE PARAPET	STD-1-1
REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS	STD-1-3
BRIDGE END DRAIN DETAILS	STD-1-4
BRIDGE END DRAIN DETAILS	STD-1-5
BRIDGE END DRAIN DETAILS	STD-1-6
STD. SEISMIC DETAILS	STD-6-1

### PROV. NO. LAST REV. DATE SPECIAL PROVISIONS

100	8-5-91	REVISIONS AND ADDITIONS TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION - MARCH 1, 1981
105A	7-21-87	APPROVAL OF SHOP DRAWINGS
604	2-18-91	CONCRETE STRUCTURES
604CX	9-16-91	CONTRACTOR - MIX DESIGN AND TESTING STRUCTURAL CONCRETE
604P	8-05-91	PRECAST PREST. BRIDGE DECK PANELS
604R	5-14-90	RIDEABILITY OF BRIDGE DECKS AND ROADWAY APPROACHES
615	12-11-89	PRECAST PREST. CONCRETE BRIDGE MEMBERS
709	5-04-87	MACHINED RIP-RAP
907A	3-25-85	EPOXY COATED REINFORCING STEEL

V = 60 MPH

BRIDGE ID NO. 28SR0150049

STATE OF TENNESSEE  
 DEPARTMENT OF TRANSPORTATION

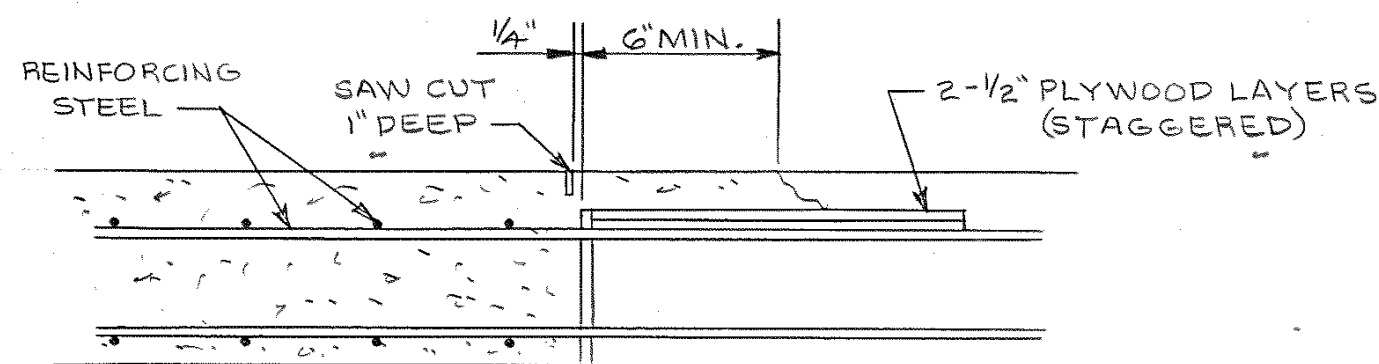
BUREAU OF HIGHWAYS

BRIDGE NO. 1  
 LAYOUT OF BRIDGE  
 STATE ROUTE 15 (US-64)  
 OVER  
 LITTLE BRADSHAW CREEK  
 STATION 132+20.67  
 GILES-LINCOLN COUNTIES  
 1991

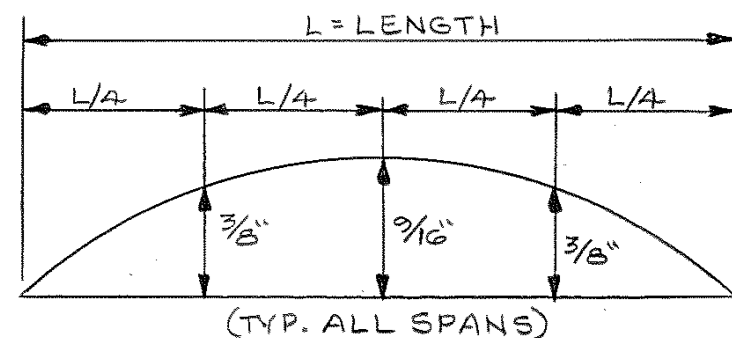
CORRECT *Edward P. Wasserman*  
 ENGINEER OF STRUCTURES

M-258-54




[illegible]

NOTE: SLAB CONSTRUCTION JOINTS MAY BE LOCATED AT THE CONTRACTOR'S OPTION EXCEPT NO JOINT MAY BE LOCATED CLOSER THAN 1/5 SPAN LENGTH FROM AN INTERIOR SUPPORT. THE CONTRACTOR SHALL MAKE ADEQUATE PROVISIONS DURING PLACEMENT OF SLAB TO PREVENT THE EXTERIOR BEAM FROM TWISTING. NO EQUIPMENT SHALL BE PERMITTED ON THE BRIDGE UNTIL ALL POURS ARE MADE AND THE CONCRETE IS PROPERLY CURED.



NOTE: THIS CURVE IS FOR DEAD LOAD SLAB AND ALL DEAD LOADS THAT ARE APPLIED AFTER SLAB IS IN PLACE AND SHOULD BE CORRECTED TO COMPENSATE FOR THE EFFECTS DUE TO VERTICAL CURVE. IF PRESTRESSED DECK PANELS ARE USED AND THE BEAMS ARE PROFILED AFTER PANELS ARE IN PLACE, REDUCE THE DEAD LOAD CORRECTION VALUES SHOWN BY 25%.

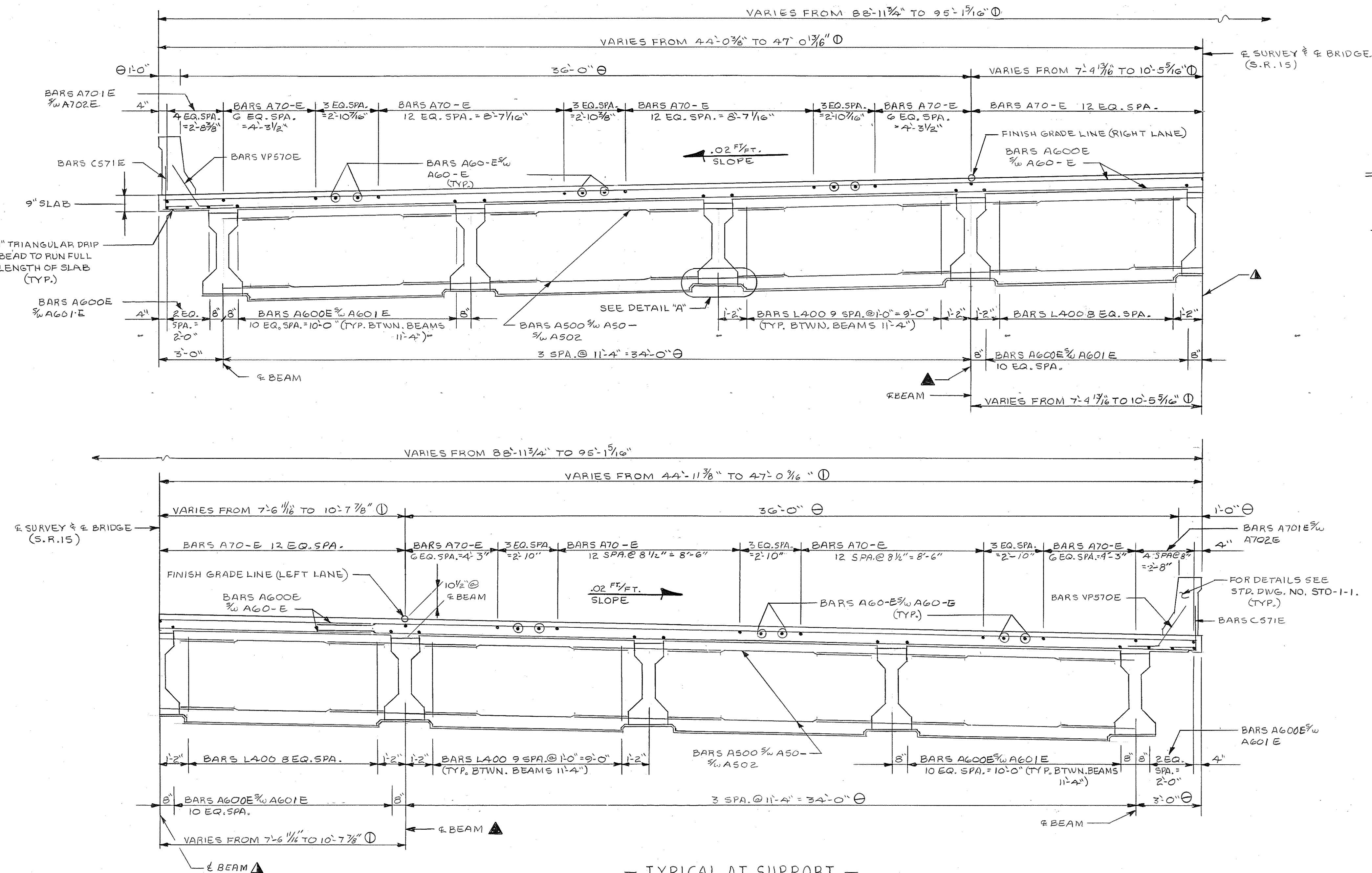
### ESTIMATED QUANTITIES

ITEM	CLASS 'A' CONCRETE (BRIDGES) C.Y.	CLASS 'D' CONCRETE (BRIDGE DECK) C.Y.	EPOXY COATED REINFORCING STEEL LBS.	STEEL BAR REINFORCEMENT (BRIDGES) LBS.
SUPERSTRUCTURE	52	489	130543 	8071

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
BUREAU OF HIGHWAYS

SUPERSTRUCTURE  
STATE ROUTE 15 (US-64)  
OVER  
LITTLE BRADSHAW CREEK  
STATION 133+32.04  
GILES & LINCOLN COUNTIES

M-258-57



\* - TYPICAL CROSS SECTION -

- ⊗ \* (LOOKING FORWARD ON SURVEY)
- ⊖ DENOTES DIMENSIONS AND BEAM SPACING PERPENDICULAR TO FG LINE (LEFT)
- ⊙ DENOTES DIMENSION PERPENDICULAR TO  $\xi$  SURVEY
- ⚠ DENOTES ALONG  $\xi$  SURVEY
- ⚠ DENOTES ALONG FINISHED GRADE LINE
- ⊕ DENOTES DIMENSIONS & BEAM SPACING ARE PERPENDICULAR TO FG LINE (RIGHT).

\* NOTE: THIS TYPICAL CROSS-SECTION VARIES FROM A NORMAL DRAWING IN THAT THE (4) FOUR BEAMS ON THE RIGHT ARE SHOWN LOOKING DOWN THE FINISHED GRADE LINE (RIGHT), THE (4) FOUR BEAMS ON THE LEFT ARE SHOWN LOOKING DOWN THE F.G. LINE (LEFT)

\* NOTE: THE (2) TWO SPACES ADJACENT TO P SURVEY ARE SHOWN, LIKE NORMAL, LOOKING FORWARD ON SURVEY.

NOTE: ALL GIRDERS TO BE SUPPORTED DURING CONSTRUCTION OF SLAB TO PREVENT ROTATION.

NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED.

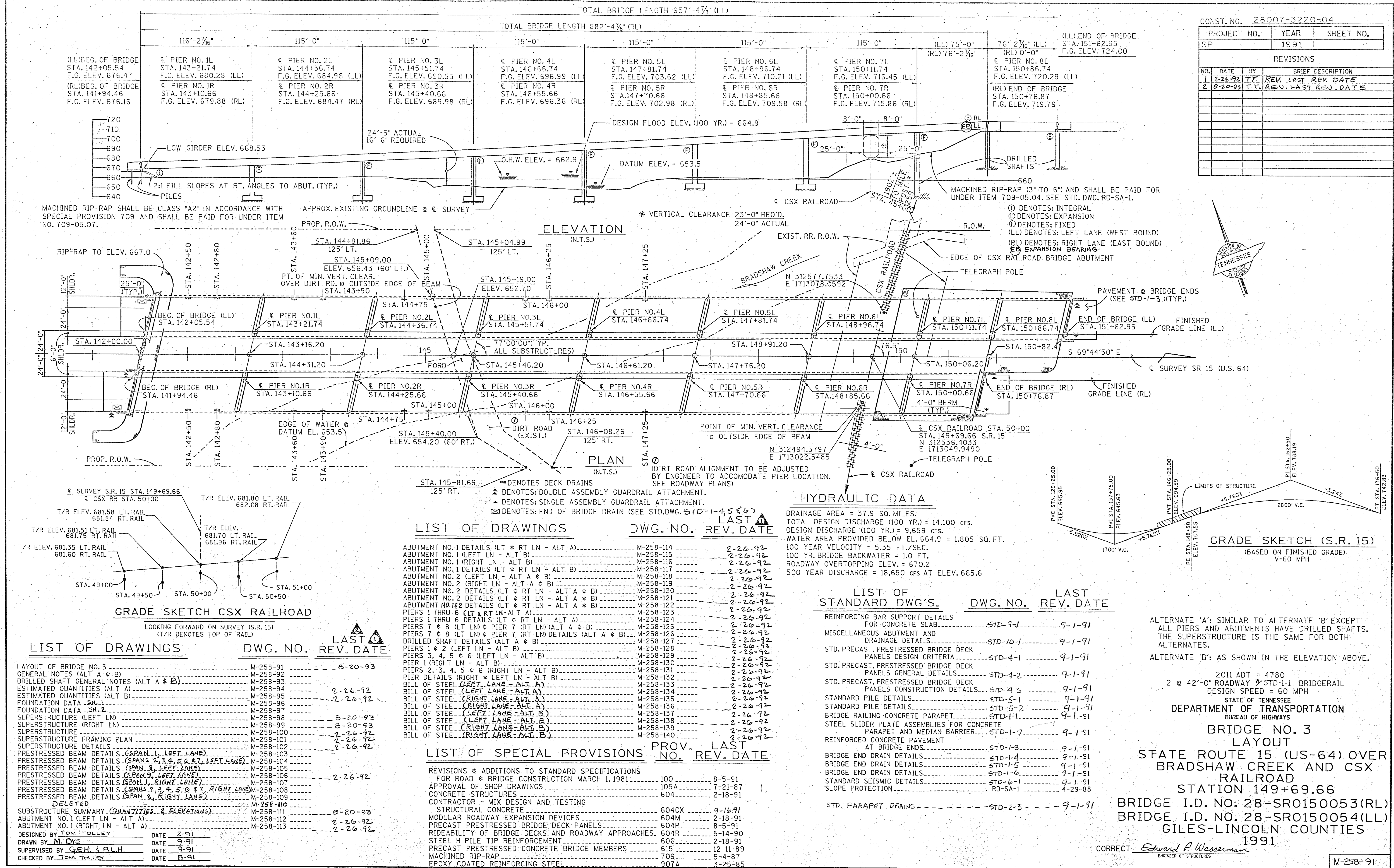
NOTE: WHEN POURING WINGWALLS, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR (WINGPOST AND) PARAPET. FOR DETAILS OF (WINGPOST AND) PARAPET SEE STANDARD DRAWING NO. STD-1-1.

NOTE: END DIAPHRAGMS OR SUPPORT DIAPHRAGMS SHALL BE POURED CONCURRENTLY WITH THE DECK SLAB AND INCLUDED IN THE QUANTITY FOR ITEM 604-03.09.

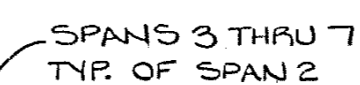
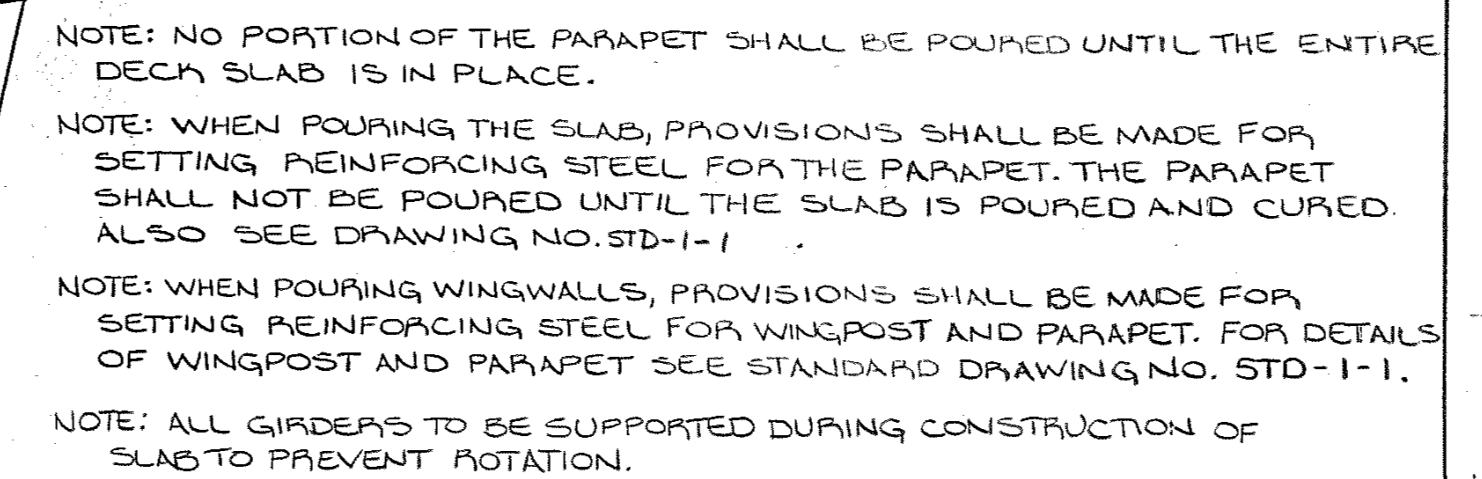
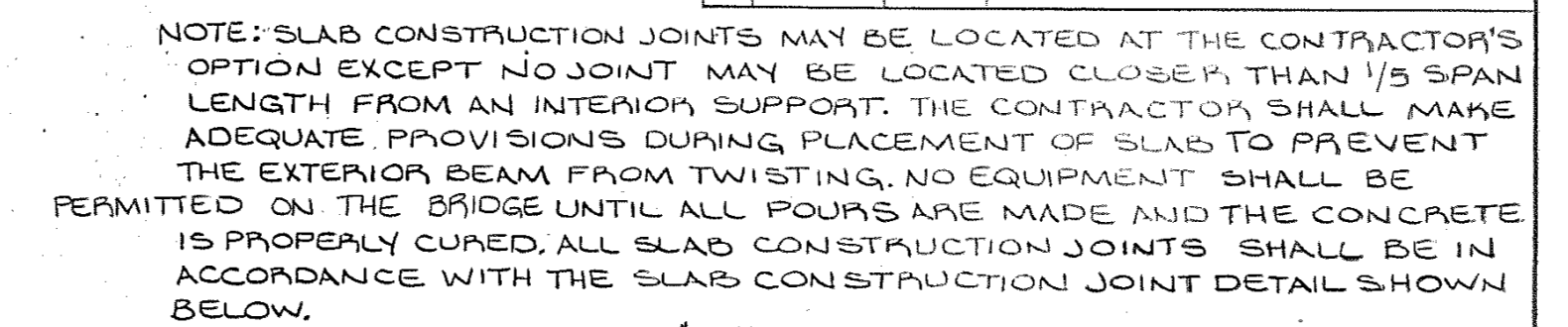
SPECIAL NOTE FOR ANCHOR BOLTS AT BENT: ANCHOR BOLT ASSEMBLIES AT BENT SHALL BE IN ACCORDANCE WITH STANDARD DRAWING NO. STD-6-1.

DESIGNED BY R. CRAWFORD DATE 5-91  
DRAWN BY FARRAR DATE 6-91  
SUPERVISED BY HARRISON & BROOKS DATE 6-91  
CHECKED BY KI DATE 9-91







[illegible]

### ESTIMATED QUANTITIES

CLASS 'A' CONC. (CY)	CLASS 'D' CONC. (BRIDGE DECK) (CY)	REIN. STEEL (LB.)	EPOXY COATED REIN. STEEL (LB.)
61	1,330	22,874	360,075

BRIDGE No. 3  
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
BUREAU OF HIGHWAYS  
SUPERSTRUCTURE  
LEFT LANE  
STATE ROUTE 15 (U.S.-64) OVER  
BRADSHAW CREEK AND C.S.X. RAILROAD  
STATION 149+69.66  
GILES - LINCOLN COUNTIES  
1991

APPROVED \_\_\_\_\_  
DIRECTOR OF HIGHWAYS

M-258-98

DESIGNED BY T. TOLLEY DATE 2/91  
DRAWN BY D.W. COOLE DATE 6/91  
SUPERVISED BY RLH - GEH DATE 6/91  
CHECKED BY T. TOLLEY DATE 7/91

\* DENOTES DIMENSION INCLUDES  
8" FOR 180° HOOK

PART PLAN OF MAIN REINFORCEMENT



[illegible]

NOTE: ALL GIRDERS TO BE SUPPORTED DURING CONSTRUCTION OF SLAB  
TO PREVENT ROTATION

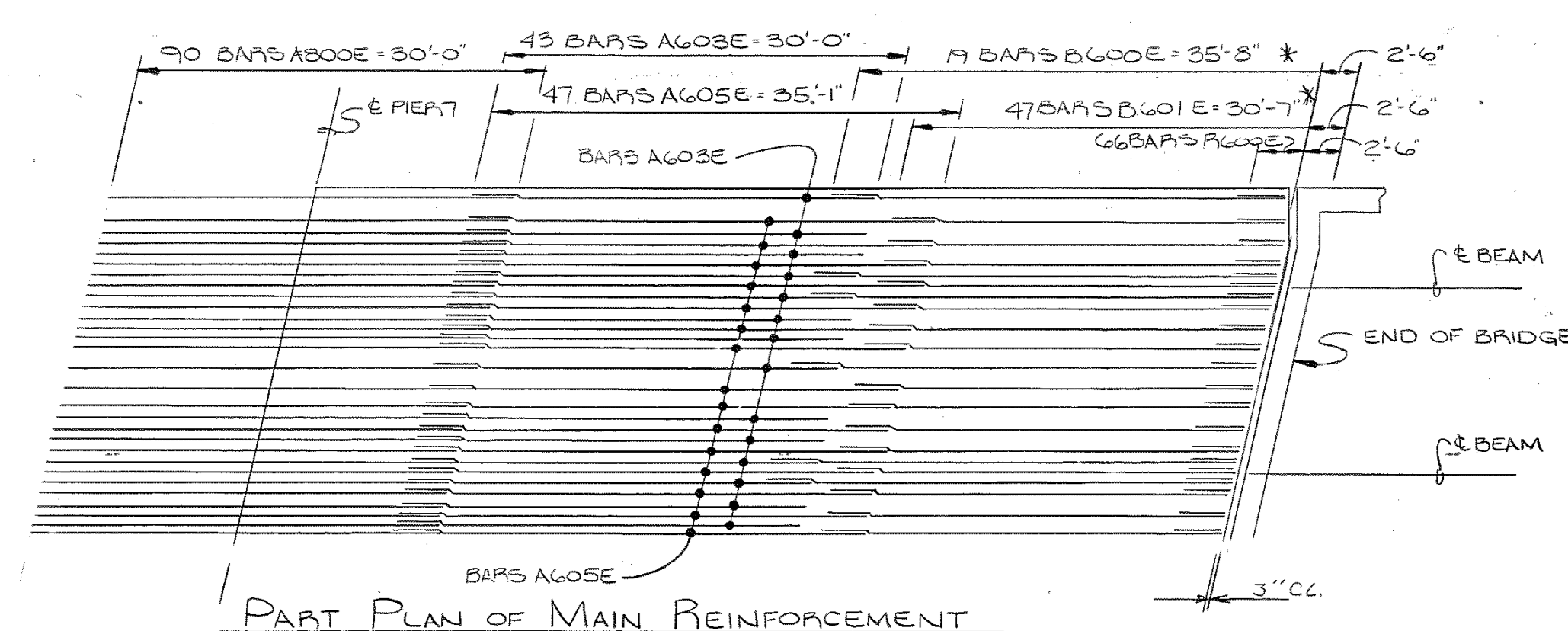
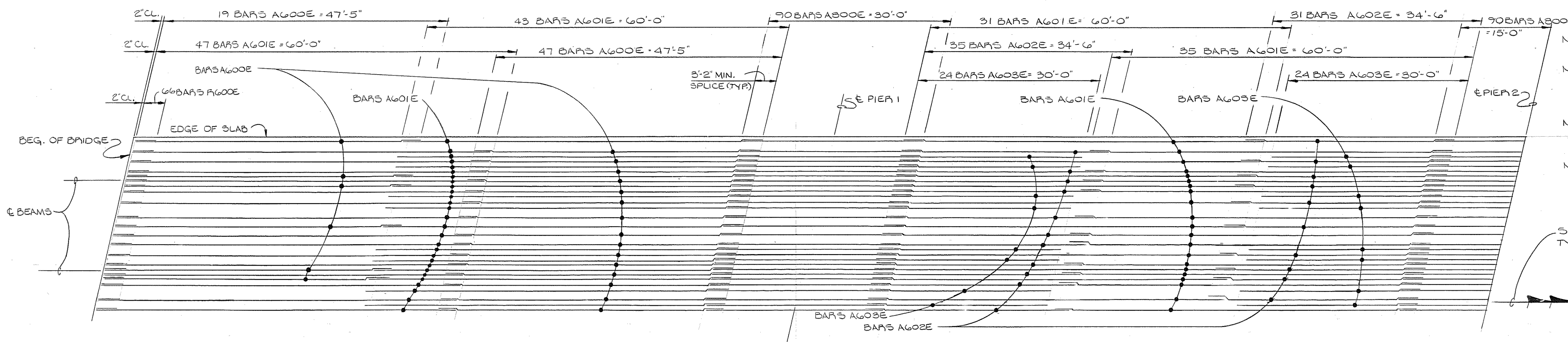
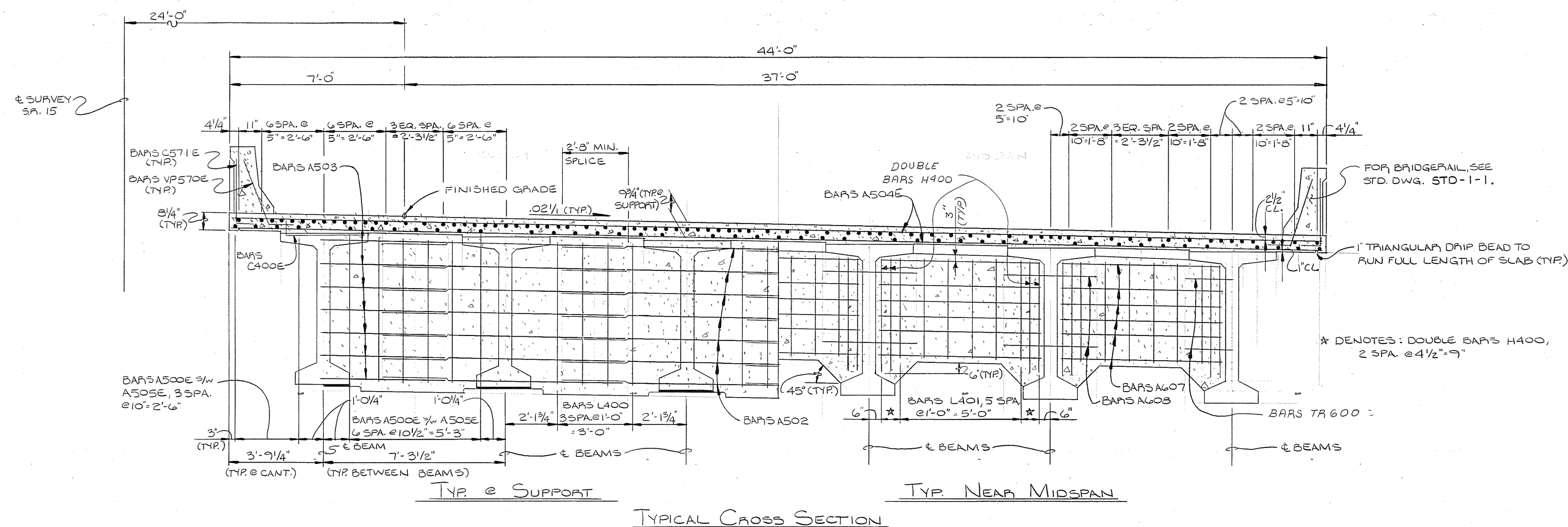
② NOTE: SEE DWG. M-258-111  
FOR POURING SEQUENCE.

CLASS 'A' CONC. (CY)	CLASS 'D' CONC. (BRIDGE DECK) (CY)	REIN. STEEL (LB.)	EPOXY COATED REIN. STEEL (LB.)
57	1,221	22,018	330,633

CORRECT Edward F. Vasseman  
ENGINEER OF STRUCTURES

APPROVED \_\_\_\_\_  
DIRECTOR OF HIGHWAYS

1-258-99

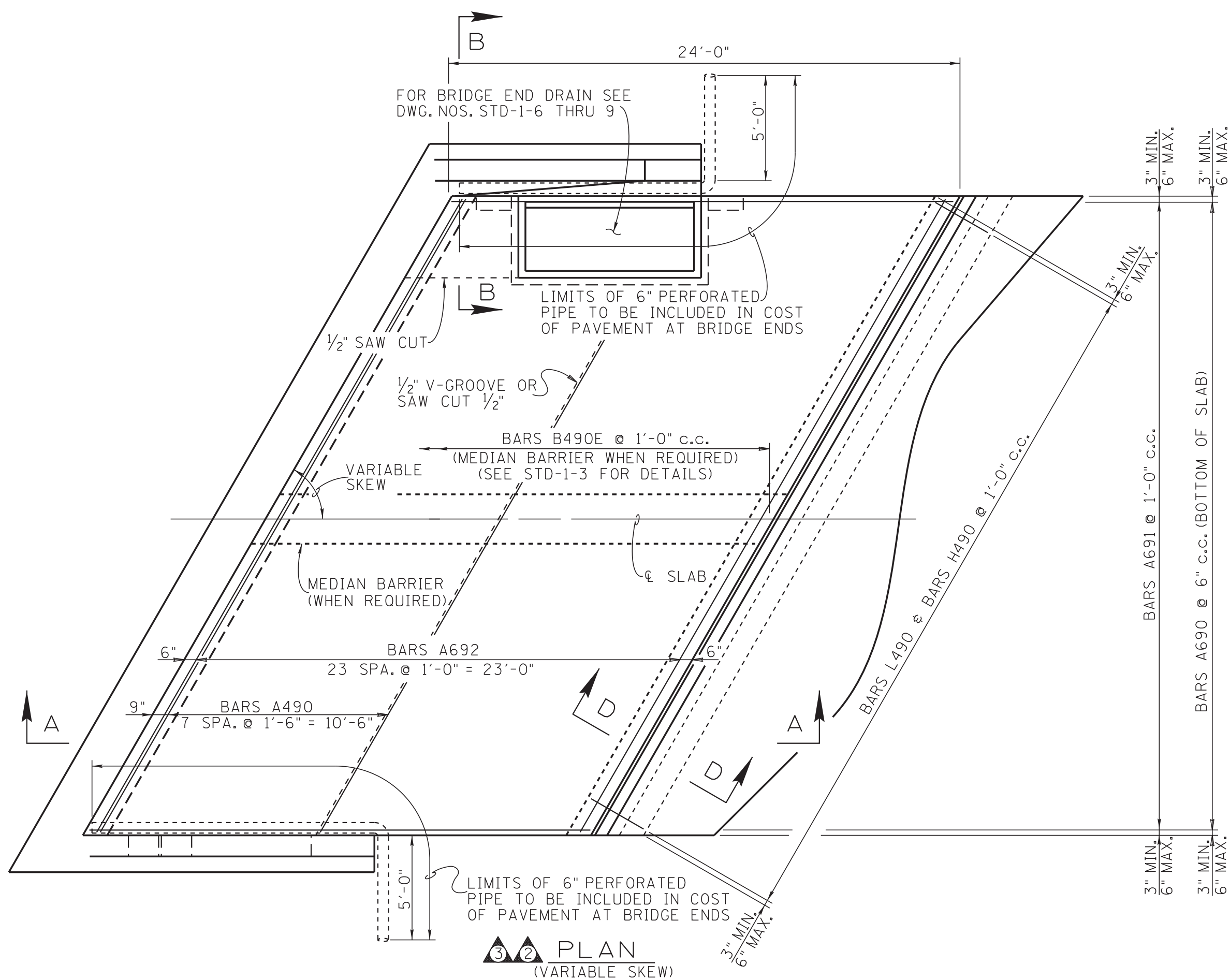
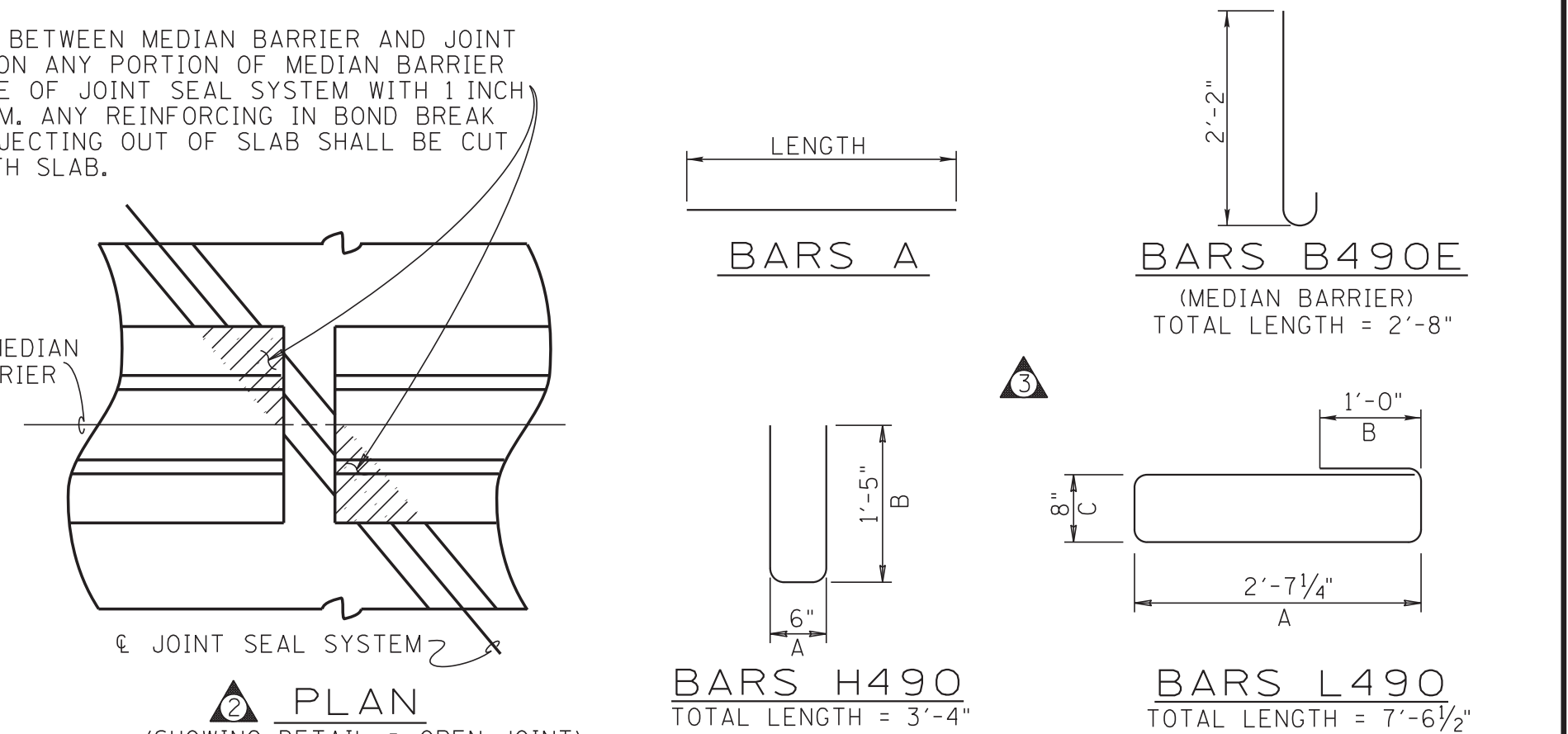
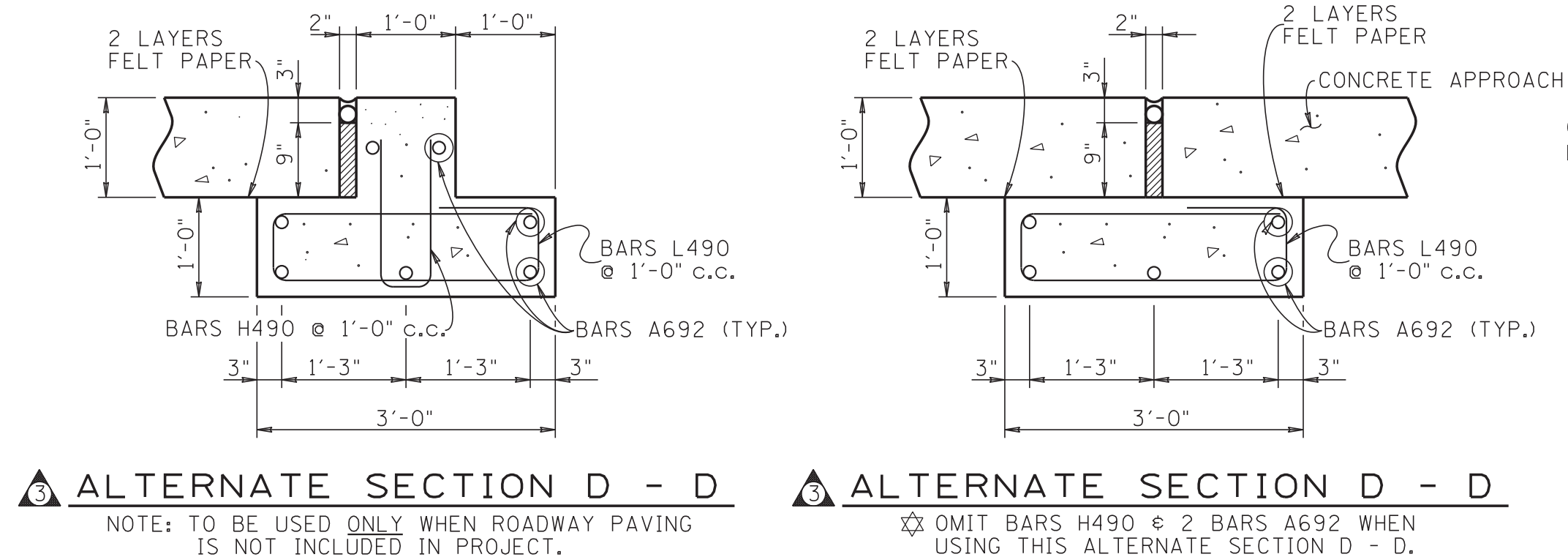
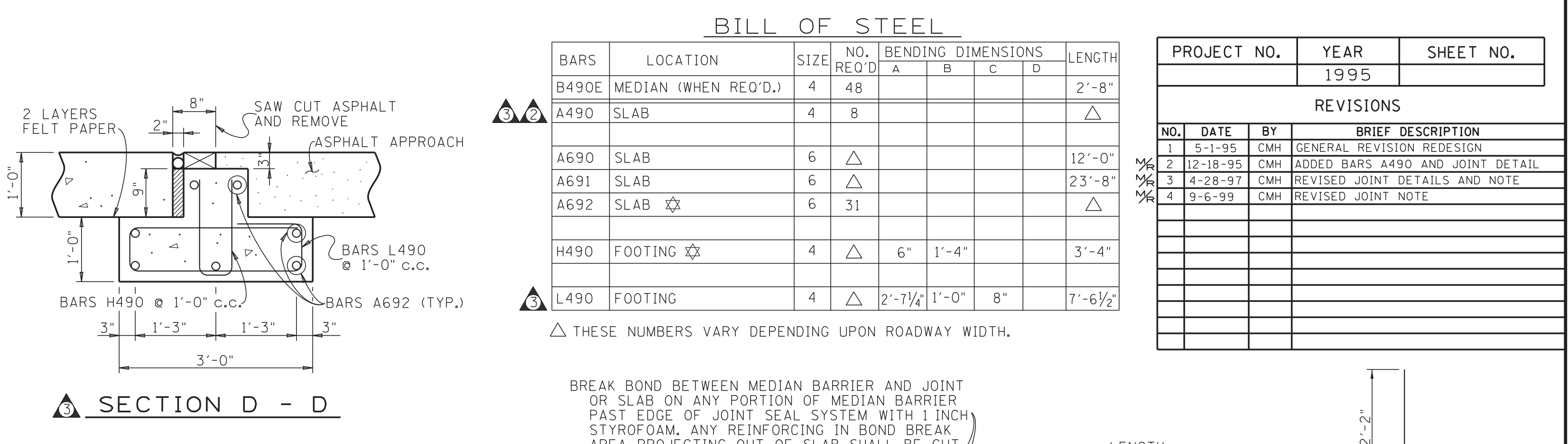


\* DENOTES DIMENSION INCLUDES  
8" FOR 180° HOOK

DESIGNED BY T. TOLLEY DATE 2/91  
DRAWN BY D.W. COBLE DATE 6/91  
SUPERVISED BY ALH - GEH DATE 6/91  
CHECKED BY T. TOLLEY DATE 7/91

CORRECT Edward F. Vasseman  
ENGINEER OF STRUCTURES





- ## NOTES
1. QUANTITIES FOR CLASS 'A' CONCRETE, REGULAR AND EPOXY COATED REINFORCING STEEL (WHEN REQUIRED FOR MEDIAN BARRIER), BACKER ROD, JOINT SEALER, ELASTOMERIC CONCRETE, STYROFOAM, GRATE AND MISCELLANEOUS MATERIALS FOR BRIDGE END DRAIN, WHEN REQUIRED, ARE TO BE INCLUDED IN PAVEMENT AT BRIDGE ENDS, S.Y. FOR BAR BENDING DIMENSIONS SEE THIS SHEET AND BILL OF STEEL FOR BRIDGE END DRAIN ON DRAWING NO. STD-1-6.
  2. COST OF MINERAL AGGREGATE CLASS A GRADING D BASE QUANTITY SHALL BE INCLUDED IN COST OF PAVEMENT AT BRIDGE ENDS. CLASS B GRADING C OR D MAY ALSO BE USED.

## GENERAL NOTES

CONCRETE: TO BE CLASS 'A' ( $f'c = 3,000$  psi)

REINFORCING STEEL: SHALL BE ASTM A615 GRADE 60 UNLESS NOTED OTHERWISE. SEE SECTION 604 AND 907 OF THE STANDARD SPECIFICATIONS.

SPECIFICATIONS: STANDARD ROAD AND BRIDGE SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION (CURRENT EDITION).

NOTE: THE APPROACH SLAB SHALL NOT BE POURED UNTIL THE ADJACENT END SPAN DECK SLAB IS IN PLACE AND ACCEPTED BY THE ENGINEER.

NOTE: THE APPROACH SLAB CONTROL ELEVATIONS SHALL BE ADJUSTED, (IF REQUIRED), SO AS TO MATCH THE IN PLACE DECK SLAB IN BOTH TRANSVERSE AND LONGITUDINAL DIRECTIONS.

④ JOINT SEAL SYSTEM:  
THE EXPANSION JOINT SYSTEM USED SHALL BE ON THE TDOT  
APPROVED QUALIFIED LISTS FOR ACCEPTABLE PRODUCTS.  
THE JOINT SYSTEM SHALL BE INSTALLED UNDER THE DIRECT  
SUPERVISION OF AN AUTHORIZED TECHNICIAN PROVIDED  
BY THE EXPANSION JOINT SUPPLIER.

M/R MINOR REVISION - FHWA APPROVAL NOT REQUIRED
--

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

 REINFORCED CONCRETE  
PAVEMENT AT BRIDGE ENDS  
1995



PLAN  
(90° SKEW)

CORRECT Edward P. Wasserman  
ENGINEER OF STRUCTURES